

STP-1020(700)X
ALTON

BEFORE SUBMITTING YOUR BID

- 1. Use pen and ink to complete the Bid.**
- 2. Have you signed and completed the Contract Agreement, Offer & Award Forms?**
- 3. As a minimum, the Bidder will submit a Bid Package consisting of the Notice to Contractors, the completed Acknowledgement of Bid Amendments & Submission of Bid Bond Validation Number form, the completed Schedule of Items, 2 copies of the completed Agreement, Offer, & Award form, a Bid Bond or Bid Guarantee, and any other Certifications or Bid Requirements listed in the Bid Book.**
- 4. Have you included prices for all Bid Items? (“Zero is not considered a bid price.”)**
- 5. Have you included a bid guarantee? Acceptable forms are:**
 - A. Bid Bond on the Department’s prescribed form for 5% of the Bid Amount. (Or forms that do not contain any significant variations from the Department’s forms as solely determined by the Department.)**
 - B. Official Bank Check, Cashier’s Check, Certified Check, U.S. Postal Money Order or Negotiable Certificate of Deposit in the amount stated in the Notice to Contractors.**
- 6. If the written Bid is to be sent, Federal Express overnight delivery is suggested as the package is delivered directly to the DOT Headquarters Building in Augusta. Other means, such as U.S. Postal Services’ Express Mail has proven not to be reliable.**

AND FOR FEDERAL AID PROJECTS

- 7. Have you included your DBE Utilization commitment in the proper amounts and signed the DBE Certification?**

If you need further information regarding Bid preparation, call the DOT Contracts Section at (207)624-3410.

For complete specifications regarding bidding requirements, refer to Section 102 of the Maine Department of Transportation, Standard Specifications, Revision December 2002.

NOTICE

The Maine Department of Transportation is attempting to improve the way Bid Amendments/Addendums are handled, and allow for an electronic downloading of bid packages from our website, while continuing to maintain a planholders list.

Prospective bidders, subcontractors or suppliers who wish to download a copy of the bid package and receive a courtesy notification of project specific bid amendments, must provide an email address to Diane Barnes at the MDOT Contracts mailbox at: MDOT.contracts@maine.gov. Each bid package will require a separate request.

Additionally, interested parties will be responsible for reviewing and retrieving the Bid Amendments from our web site, and acknowledging receipt and incorporating those Bid Amendments in their bids using the Acknowledgement of Bid Amendment Form.

The downloading of bid packages from the MDOT website is not the same as providing an electronic bid to the Department. Electronic bids must be submitted via <http://www.BIDX.com>. For information on electronic bidding contract Rebecca Pooler at rebecca.pooler@maine.gov.

NOTICE

For security and other reasons, all Bid Packages which are mailed, shall be provided in double (one envelope inside the other) envelopes. The *Inner Envelope* shall have the following information provided on it:

Bid Enclosed - Do Not Open

PIN:

Town:

Date of Bid Opening:

Name of Contractor with mailing address and telephone number:

In Addition to the usual address information, the *Outer Envelope* should have written or typed on it:

Double Envelope: Bid Enclosed

PIN:

Town:

Date of Bid Opening:

Name of Contractor:

This should not be much of a change for those of you who use Federal Express or similar services.

Hand-carried Bids may be in one envelope as before, and should be marked with the following information:

Bid Enclosed: Do Not Open

PIN:

Town:

Name of Contractor:

October 16, 2001

STATE OF MAINE DEPARTMENT OF TRANSPORTATION
Bid Guaranty-Bid Bond Form

KNOW ALL MEN BY THESE PRESENTS THAT_____

_____, of the City/Town of _____ and State of _____

as Principal, and _____ as Surety, a

Corporation duly organized under the laws of the State of _____ and having a usual place of

Business in _____ and hereby held and firmly bound unto the Treasurer of

the State of Maine in the sum of _____ for payment which Principal and Surety bind

themselves, their heirs, executors, administrators, successors and assigns, jointly and severally.

The condition of this obligation is that the Principal has submitted to the Maine Department of

Transportation, hereafter Department, a certain bid, attached hereto and incorporated as a

part herein, to enter into a written contract for the construction of _____

_____ and if the Department shall accept said bid

and the Principal shall execute and deliver a contract in the form attached hereto (properly

completed in accordance with said bid) and shall furnish bonds for this faithful performance of

said contract, and for the payment of all persons performing labor or furnishing material in

connection therewith, and shall in all other respects perform the agreement created by the

acceptance of said bid, then this obligation shall be null and void; otherwise it shall remain in full

force, and effect.

Signed and sealed this _____ day of _____ 20_____

WITNESS:

WITNESS

PRINCIPAL:

By _____

By: _____

By: _____

SURETY:

By _____

By: _____

Name of Local Agency: _____

NOTICE

Bidders:

Please use the attached “Request for Information” form when faxing questions and comments concerning specific Contracts that have been Advertised for Bid. Include additional numbered pages as required.

REQUEST FOR INFORMATION

This image shows a blank sheet of white paper with horizontal ruling lines. The lines are evenly spaced and run across the width of the page. There are no margins, text, or other markings on the paper.

[illegible]

Response By:_____ Date:_____

INSTRUCTIONS FOR PREPARING THE CONTRACTOR'S DISADVANTAGED BUSINESS ENTERPRISE UTILIZATION PLAN

The Contractor Shall:

1. Submit a completed Contractor's Disadvantaged Business Enterprise Utilization Plan to the Contract's Engineer by 4:30 P.M. on the Bid day.
2. Extend equal opportunity to MDOT certified DBE firms (as listed in MDOT's DBE Directory of Certified Businesses) in the selection and utilization of Subcontractors and Suppliers.

SPECIFIC INSTRUCTIONS FOR COMPLETING THE FORM:

Insert Contractor name, the name of the person(s) preparing the form, and that person(s) telephone and fax number.

Provide total Bid price, Federal Project Identification Number, and location of the Project work.

In the columns, name each DBE firm to be used, provide the Unit or Item cost of the Work/Product to be provided by the DBE firm, give a brief description of the Work, and the dollar value of the Work.

If no DBE firm is to be utilized, the Contractor must document the reason(s) why no DBE firms are being used. Specific supporting evidence of good faith efforts taken by Contractors to solicit DBE Bidders must be attached. This evidence, as a minimum, includes phone logs, e-mail and/or mail DBE solicitation records, and the documented results of these solicitations.

NOTICE

Disadvantaged Business Enterprise Proposed Utilization

The Apparent Low Bidder must submit the Disadvantaged Business Enterprise Proposed Utilization form by close of Business (4:30 P.M.) on Bid day.

The Contractor's Disadvantaged Business Enterprise Proposed Utilization Plan form contains additional information that is required by USDOT.

The Contractor's Disadvantaged Business Enterprise Proposed Utilization Plan form must be used.

A copy of the new Contractor's Disadvantaged Business Enterprise Proposed Utilization Plan and instructions for completing it are attached.

Note: Questions about DBE firms, or to obtain a printed copy of the DBE Directory, contact Equal Opportunity at (207) 624-3066.

MDOT's DBE Directory of Certified firms can also be obtained at http://www.state.me.us/mdot/humnres/o_equalo/cdwbed_h.htm

CONTRACTOR'S DISADVANTAGED BUSINESS ENTERPRISE PROPOSED UTILIZATION PLAN

Low Bidder shall furnish completed form to Contracts Section by 4:30 P.M. on Bid Opening day.

TO: MDOT Contracts Section
16 State House Station,
Augusta, Me 04333-0016
or
Fax: 207-624-3431

Contractor: _____

Prepared by: _____

Telephone: _____ Fax: _____

BID PRICE: \$ _____ FEDERAL PROJECT # _____ LOCATION: _____

TOTAL DBE PARTICIPATION AS A PERCENT OF TOTAL BID PRICE = _____ %

DBE Firm*	Unit/Item Cost	Unit #	Description of work & Item Number	Actual \$ Value
Total >				

If no DBE firm(s) are used, bidder must document efforts made to secure DBE participation and attach supporting evidence of this effort:

_____.

Examples: Bidder relies wholly upon low quote subcontractor section, DBE firm(s) were not low quote.
No DBE firms bid.

*Only DBE firms certified by MDOT prior to bidding can be utilized by Contractor for DBE credit.
Directory of certified DBEs is available on MDOT's website: www.state.me.us/mdot

Equal Opportunity Use:

Plan received ____/____/____ Verified by: _____ Action: _____



Office of Human Resources

Equal Opportunity

MAINE DEPARTMENT OF TRANSPORTATION

Certified Disadvantaged and Women Business Enterprise

DBE DIRECTORY - MINORITY OWNED

WBE DIRECTORY - WOMEN OWNED

WEBSITE FOR DIRECTORY CAN BE FOUND AT:

http://www.state.me.us/mdot/humnres/o_equalo/cdwbed_h.htm

It is the responsibility of the Contractor to access the DBE Directory at this site in order to have the most current listings.

STATE OF MAINE DEPARTMENT OF TRANSPORTATION NOTICE TO CONTRACTORS

Sealed Bids addressed to the Maine Department of Transportation, Augusta, Maine 04333 and endorsed on the wrapper "Bid for the **Hot Mix Asphalt Overlay, Plant Mixed Recycled Asphalt Pavement, Variable Depth Gravel Areas, Full Construction Areas, Drainage and Safety Improvements** in the town of **Alton**" will be received from contractors at the Reception Desk, Maine DOT Building, Child Street, Augusta, Maine, until 11:00 o'clock A.M. (prevailing time) on **April 28, 2004**, and at that time and place publicly opened and read. Bids will be accepted from contractors prequalified by the Department of Transportation for highway construction or paving projects. All other Bids may be rejected. **MDOT provides the option of electronic bidding. We accept electronic bids for those bid packages posted on the bidx.com website. Electronic bids do not have to be accompanied by paper bids. Please note: the Department will accept a facsimile of the bid bond; however, the original bid bond must then be received at the MDOT Contract Section within 72 hours of the bid opening. Until further notice, dual bids (one paper, one electronic) will be accepted, with the paper copy taking precedence.**

Description: Maine Federal Aid Project No. STP-1020(700)X, PIN 10207.00

Location: In Penobscot County, project STP-1020(700)X is located on Route 16, beginning 1.37 km (0.85 mi) southerly of the LaGrange/Alton town line and extending southerly 7.235 km (4.50 mi).

Outline of Work: Hot Mix Asphalt Overlay, Plant Mixed Recycled Asphalt Pavement, Variable Gravel Areas, Full Construction Areas, and Drainage and Safety Improvements and other incidental work.

For general information regarding Bidding and Contracting procedures, contact Scott Bickford at (207)624-3410. Our webpage at <http://www.state.me.us/mdot/project/design/homepg.htm> contains a copy of the schedule of items, Plan Holders List, written portions of bid amendments (not drawings), and bid results. For Project-specific information fax all questions to **James Andrews** at (207)624-3401. Questions received after 12:00 noon of Monday prior to bid date will not be answered. Bidders shall not contact any other Departmental staff for clarification of Contract provisions, and the Department will not be responsible for any interpretations so obtained. Hearing impaired persons may call the Telecommunication Device for the Deaf at (207) 624-3007.

Plans, specifications and bid forms may be seen at the Maine DOT Building in Augusta, Maine. They may be purchased from the Department between the hours of 8:00 a.m. to 4:30 p.m. by cash, credit card (Visa/Mastercard) or check payable to Treasurer, State of Maine sent to Maine Department of Transportation, Attn.: Mailroom, 16 State House Station, Augusta, Maine 04333-0016. They also may be purchased by telephone at (207)624-3536 between the hours of 8:00 a.m. to 4:30 p.m. Full size plans \$97.00 (\$103.00 by mail). Half size plans \$48.50 (\$52.00 by mail). Bid Book \$10 (\$13 by mail), Single Sheets \$2, payment in advance, all non-refundable.

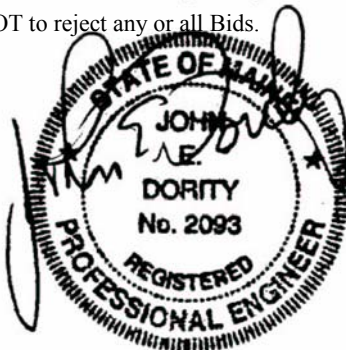
Each Bid must be made upon blank forms provided by the Department and must be accompanied by a bid bond at 5% of the bid amount or an official bank check, cashier's check, certified check, certificate of deposit, or United States postal money order in the amount of \$60,000 payable to Treasurer, State of Maine as a Bid guarantee. A Contract Performance Surety Bond and a Contract Payment Surety Bond, each in the amount of 100 percent of the Contract price, will be required of the successful Bidder.

This Contract is subject to all applicable Federal Laws. This contract is subject to compliance with the Disadvantaged Business Enterprise program requirements as set forth by the Maine Department of Transportation.

All work shall be governed by "State of Maine, Department of Transportation, Standard Specifications, Revision of December 2002", price \$10 [\$13 by mail], and Standard Details, Revision of December 2002, price \$20 [\$25 by mail] Standard Detail updates can be found at <http://www.state.me.us/mdot/project/design/homepg.htm>

The right is hereby reserved to the MDOT to reject any or all Bids.

Augusta, Maine
April 7, 2004



JOHN E. DORITY
CHIEF ENGINEER

SPECIAL PROVISION 102.7.3
ACKNOWLEDGMENT OF BID AMENDMENTS
&
SUBMISSION OF BID BOND VALIDATION NUMBER (IF APPLICABLE)

With this form, the Bidder acknowledges its responsibility to check for all Amendments to the Bid Package. For each Project under Advertisement, Amendments are located at <http://www.maine.gov/mdot/comprehensive-list-projects/project-information.php>. It is the responsibility of the Bidder to determine if there are Amendments to the Project, to download them, to incorporate them into their Bid Package, and to reference the Amendment number and the date on the form below. The Maine DOT will not post Bid Amendments any later than noon the day before Bid opening without individually notifying all the planholders.

Amendment Number	Date

The Contractor, for itself, its successors and assigns, hereby acknowledges that it has received all of the above referenced Amendments to the Bid Package.

CONTRACTOR

Date

Signature of authorized representative

(Name and Title Printed)

MAINE DEPARTMENT OF TRANSPORTATION

BID

DATE OF OPENING :

CALL ORDER :

CONTRACT ID : 010207.00

PROJECTS

STP-1020 (700) X

COUNTY : PENOBSCOT

MAINE DEPARTMENT OF TRANSPORTATION

PAGE: 1

SCHEDULE OF ITEMS

DATE: 040330

REVISED:

CONTRACT ID: 010207.00

PROJECT(S): STP-1020(700)X

CONTRACTOR : _____

LINE NO	ITEM DESCRIPTION	APPROX. QUANTITY AND UNITS	UNIT PRICE		BID AMOUNT	
			DOLLARS	CTS	DOLLARS	CTS

SECTION 0001 HIGHWAY ITEMS

0010	201.23 REMOVING SINGLE TREE TOP ONLY	6.000 EA				
0020	202.203 PAVEMENT BUTT JOINTS	75.000 M2				
0030	203.2001 COMMON EXCAVATION - PLAN QUANTITY	11130.000 M3				
0040	211.21 INSLOPE REHABILITATION	4370.000 M				
0050	211.30 DITCH EXCAVATION	2220.000 M				
0060	211.40 NEW DITCH EXCAVATION	970.000 M				
0070	304.104 AGGREGATE SUBBASE COURSE - GRAVEL (PLAN QUANTITY)	16200.000 M3				
0080	310.23 PLANT MIX RECYCLED ASPHALT PAVEMENT - 75 MM DEPTH	62210.000 M2				
0090	403.209 HOT MIX ASPHALT 9.5 MM(SIDEWALKS, DRIVES, INCIDENTAL)	460.000 MG				

MAINE DEPARTMENT OF TRANSPORTATION

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SCHEDULE OF ITEMS

DATE: 040330

REVISED:

CONTRACT ID: 010207.00

PROJECT(S): STP-1020(700)X

CONTRACTOR : _____

LINE NO	ITEM DESCRIPTION	APPROX. QUANTITY AND UNITS	UNIT PRICE DOLLARS	CTS	BID AMOUNT DOLLARS	CTS
0100	403.210 HOT MIX ASPHALT 9.5 MM NOMINAL MAX SIZE	4630.000 MG				
0110	403.213 HOT MIX ASPHALT 12.5 MM, BASE	6940.000 MG				
0120	409.15 BITUMINOUS TACK COAT APPLIED	10120.000 L				
0130	411.10 UNTREATED AGGREGATE SURFACE COURSE (TRUCK MEASURE)	80.000 M3				
0140	603.16 375 MM CULVERT PIPE OPTION I	220.000 M				
0150	603.179 450 MM CULVERT PIPE OPTION III	43.000 M				
0160	603.19 600 MM CULVERT PIPE OPTION I	21.200 M				
0170	603.199 600 MM CULVERT PIPE OPTION III	18.000 M				
0180	603.289 2100 MM CULVERT PIPE OPTION III	6.200 M				
0190	603.7315 REMOVE & RELAY 375 MM METAL PIPE	30.000 M				
0200	604.092 CATCH BASIN TYPE B1-C	1.000 EA				

MAINE DEPARTMENT OF TRANSPORTATION

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SCHEDULE OF ITEMS

DATE: 040330

REVISED:

CONTRACT ID: 010207.00

PROJECT(S): STP-1020(700)X

CONTRACTOR : _____

LINE NO	ITEM DESCRIPTION	APPROX. QUANTITY AND UNITS	UNIT PRICE DOLLARS	CTS	BID AMOUNT DOLLARS	CTS
0210	604.244 CATCH BASIN TYPE F4	1.000 EA				
0220	605.09 150 MM UNDERDRAIN TYPE B	900.000 M				
0230	606.23 GUARDRAIL TYPE 3C - SINGLE RAIL	1700.000 M				
0240	606.231 GUARDRAIL TYPE 3C - 4.5 M RADIUS AND LESS	16.000 M				
0250	606.232 GUARDRAIL TYPE 3C - OVER 4.5 M RADIUS	77.000 M				
0260	606.265 TERMINAL END - SINGLE RAIL - GALVANIZED STEEL	12.000 EA				
0270	606.35 GUARDRAIL DELINEATOR POST	57.000 EA				
0280	606.364 GUARDRAIL REMOVE, MODIFY AND RESET, TYPE 3B	100.000 M				
0290	606.47 SINGLE WOOD POST	20.000 EA				
0300	606.754 WIDEN SHOULDER FOR 350 END TREATMENT	21.000 EA				
0310	606.79 GUARDRAIL 350 FLARED TERMINAL	21.000 EA				

MAINE DEPARTMENT OF TRANSPORTATION

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SCHEDULE OF ITEMS

DATE: 040330

REVISED:

CONTRACT ID: 010207.00

PROJECT(S): STP-1020(700)X

CONTRACTOR : _____

LINE NO	ITEM DESCRIPTION	APPROX. QUANTITY AND UNITS	UNIT PRICE		BID AMOUNT	
			DOLLARS	CTS	DOLLARS	CTS
0320	609.31 CURB TYPE 3	850.000				
		M				
0330	610.08 PLAIN RIPRAP	140.000				
		M3				
0340	613.319 EROSION CONTROL BLANKET	6460.000				
		M2				
0350	615.07 LOAM	50.000				
		M3				
0360	618.1301 SEEDING METHOD NUMBER 1 - PLAN QUANTITY	5.000				
		UN				
0370	618.1401 SEEDING METHOD NUMBER 2 - PLAN QUANTITY	470.000				
		UN				
0380	619.1201 MULCH - PLAN QUANTITY	475.000				
		UN				
0390	620.58 EROSION CONTROL GEOTEXTILE	312.000				
		M2				
0400	627.75 WHITE OR YELLOW PAVEMENT AND CURB MARKING	3.000				
		M2				
0410	627.76 TEMPORARY PAVEMENT MARKING LINE, WHITE OR YELLOW	LUMP	LUMP			
0420	629.05 HAND LABOR, STRAIGHT TIME	40.000				
		HR				

MAINE DEPARTMENT OF TRANSPORTATION

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SCHEDULE OF ITEMS

DATE: 040330

REVISED:

CONTRACT ID: 010207.00

PROJECT(S): STP-1020(700)X

CONTRACTOR : _____

LINE NO	ITEM DESCRIPTION	APPROX. QUANTITY AND UNITS	UNIT PRICE DOLLARS	CTS	BID AMOUNT DOLLARS	CTS
0430	631.10 AIR COMPRESSOR (INCLUDING OPERATOR)	10.000 HR				
0440	631.11 AIR TOOL (INCLUDING OPERATOR)	10.000 HR				
0450	631.12 ALL PURPOSE EXCAVATOR (INCLUDING OPERATOR)	40.000 HR				
0460	631.172 TRUCK - LARGE (INCLUDING OPERATOR)	80.000 HR				
0470	631.18 CHAIN SAW RENTAL (INCLUDING OPERATOR)	30.000 HR				
0480	631.20 STUMP CHIPPER (INCLUDING OPERATOR)	10.000 HR				
0490	631.32 CULVERT CLEANER (INCLUDING OPERATOR)	30.000 HR				
0500	639.19 FIELD OFFICE TYPE B	1.000 EA				
0510	652.38 FLAGGER	4400.000 HR				
0520	652.39 WORK ZONE TRAFFIC CONTROL	LUMP	LUMP			
0530	656.75 TEMPORARY SOIL EROSION AND WATER POLLUTION CONTROL	LUMP	LUMP			

MAINE DEPARTMENT OF TRANSPORTATION

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SCHEDULE OF ITEMS

DATE: 040330

REVISED:

CONTRACT ID: 010207.00

PROJECT(S): STP-1020(700)X

CONTRACTOR : _____

LINE NO	ITEM DESCRIPTION	APPROX. QUANTITY AND UNITS	UNIT PRICE DOLLARS CTS	BID AMOUNT DOLLARS CTS
0540	659.10 MOBILIZATION	LUMP	LUMP	
0550	660.21 ON-THE-JOB TRAINING (BID)	1000.000 HR		
	SECTION 0001 TOTAL			
	TOTAL BID			

CONTRACT AGREEMENT, OFFER & AWARD

AGREEMENT made on the date last signed below, by and between the State of Maine, acting through and by its Department of Transportation (Department), an agency of state government with its principal administrative offices located at Child Street, Augusta, Maine, with a mailing address at 16 State House Station, Augusta, Maine 04333-0016, and

_____ a corporation or other legal entity organized under the laws of the State of Maine, with its principal place of business located at _____

The Department and the Contractor, in consideration of the mutual promises set forth in this Agreement (the "Contract"), hereby agree as follows:

A. The Work.

The Contractor agrees to complete all Work as specified or indicated in the Contract including Extra Work in conformity with the Contract, **STP-1020(700)X, PIN 10207.00, for the Hot Mix Asphalt Overlay, Plant Mix Recycled Asphalt Pavement, Variable Depth Gravel Areas, Full Construction Areas, Drainage and Safety Improvements in the town of Alton, County of Penobscot, Maine.** The Work includes construction, maintenance during construction, warranty as provided in the Contract, and other incidental work.

The Contractor shall be responsible for furnishing all supervision, labor, equipment, tools supplies, permanent materials and temporary materials required to perform the Work including construction quality control including inspection, testing and documentation, all required documentation at the conclusion of the project, warranting its work and performing all other work indicated in the Contract.

The Department shall have the right to alter the nature and extent of the Work as provided in the Contract; payment to be made as provided in the same.

B. Time.

The Contractor agrees to complete all Work, except warranty work, on or before **October 30, 2004.** Further, the Department may deduct from moneys otherwise due the Contractor, not as a penalty, but as Liquidated Damages in accordance with Sections 107.7 and 107.8 of the State of Maine Department of Transportation Standard Specifications, Revision of December 2002.

C. Price.

The quantities given in the Schedule of Items of the Bid Package will be used as the basis for determining the original Contract amount and for determining the amounts of the required Performance Surety Bond and Payment Surety Bond, and that the amount of this offer is _____

\$_____ Performance Bond and Payment Bond each being 100% of the amount of this Contract.

D. Contract.

This Contract, which may be amended, modified, or supplemented in writing only, consists of the Contract documents as defined in the Plans, Standard Specifications, Revision of December 2002, Standard Details Revision of December 2002 as updated through advertisement, Supplemental Specifications, Special Provisions, Contract Agreement; and Contract Bonds. It is agreed and understood that this Contract will be governed by the documents listed above.

E. Certifications.

By signing below, the Contractor hereby certifies that to the best of the Contractor's knowledge and belief:

1. All of the statements, representations, covenants, and/or certifications required or set forth in the Bid and the Bid Documents, including those in Appendix A to Division 100 of the Standard Specifications Revision of December 2002 (Federal Contract Provisions Supplement), and the Contract are still complete and accurate as of the date of this Agreement.
2. The Contractor knows of no legal, contractual, or financial impediment to entering into this Contract.
3. The person signing below is legally authorized by the Contractor to sign this Contract on behalf of the Contractor and to legally bind the Contractor to the terms of the Contract.

F. Offer.

The undersigned, having carefully examined the site of work, the Plans, Standard Specifications Revision of December 2002, Standard Details Revision of December 2002 as updated through advertisement, Supplemental Specifications, Special Provisions, Contract Agreement; and Contract Bonds contained herein for construction of: **STP-1020(700)X, PIN 10207.00, for the Hot Mix Asphalt Overlay, Plant Mix Recycled Asphalt Pavement, Variable Depth Gravel Areas, Full Construction Areas, Drainage and Safety Improvements in the town of Alton, County of Penobscot, Maine** on which bids will be received until the time specified in the “Notice to Contractors” do(es) hereby bid and offer to enter into this contract to supply all the materials, tools, equipment and labor to construct the whole of the Work in strict accordance with the terms and conditions of this Contract at the unit prices in the attached “Schedule of Items”.

The Offeror agrees to perform the work required at the price specified above and in accordance with the bids provided in the attached “Schedule of Items” in strict accordance with the terms of this solicitation, and to provide the appropriate insurance and bonds if this offer is accepted by the Government in writing.

As Offeror also agrees:

First: To do any extra work, not covered by the attached “Schedule of Items”, which may be ordered by the Resident, and to accept as full compensation the amount determined upon a “Force Account” basis as provided in the Standard Specifications, Revision of December 2002, and as addressed in the contract documents.

Second: That the bid bond at 5% of the bid amount or the official bank check, cashier’s check, certificate of deposit or U. S. Postal Money Order in the amount given in the “Notice to Contractors”, payable to the Treasurer of the State of Maine and accompanying this bid, shall be forfeited, as liquidated damages, if in case this bid is accepted, and the undersigned shall fail to abide by the terms and conditions of the offer and fail to furnish satisfactory insurance and Contract bonds under the conditions stipulated in the Specifications within 15 days of notice of intent to award the contract.

Third: To begin the Work on the date specified in the Engineer’s “Notice to Commence Work” as stated in Section 107.2 of the Standard Specifications Revision of December 2002 and complete the Work within the time limits given in the Special Provisions of this Contract.

Fourth: The Contractor will be bound to the Disadvantaged Business Enterprise (DBE) Requirements contained in the attached Notice (Additional Instructions to Bidders) and submit a completed Contractor’s Disadvantaged Business Enterprise Utilization Plan by 4:30pm on the day of bid opening to the Contracts Engineer.

Fifth: That this offer shall remain open for 30 calendar days after the date of opening of bids.

Sixth: The Bidder hereby certifies, to the best of its knowledge and belief that: the Bidder has not, either directly or indirectly, entered into any agreement, participated in any collusion, or otherwise taken any action in restraint of competitive bidding in connection with its bid, and its subsequent contract with the Department.

IN WITNESS WHEREOF, the Contractor, for itself, its successors and assigns, hereby execute two duplicate originals of this Agreement and thereby binds itself to all covenants, terms, and obligations contained in the Contract Documents.

CONTRACTOR

Date

(Signature of Legally Authorized Representative
of the Contractor)

Witness

(Name and Title Printed)

G. Award.

Your offer is hereby accepted.
documents referenced herein.

This award consummates the Contract, and the

MAINE DEPARTMENT OF TRANSPORTATION

Date

By: David A. Cole, Commissioner

Witness

CONTRACT AGREEMENT, OFFER & AWARD

AGREEMENT made on the date last signed below, by and between the State of Maine, acting through and by its Department of Transportation (Department), an agency of state government with its principal administrative offices located at Child Street, Augusta, Maine, with a mailing address at 16 State House Station, Augusta, Maine 04333-0016, and

_____ a corporation or other legal entity organized under the laws of the State of Maine, with its principal place of business located at _____

The Department and the Contractor, in consideration of the mutual promises set forth in this Agreement (the "Contract"), hereby agree as follows:

A. The Work.

The Contractor agrees to complete all Work as specified or indicated in the Contract including Extra Work in conformity with the Contract, **STP-1020(700)X, PIN 10207.00, for the Hot Mix Asphalt Overlay, Plant Mix Recycled Asphalt Pavement, Variable Depth Gravel Areas, Full Construction Areas, Drainage and Safety Improvements in the town of Alton, County of Penobscot, Maine.** The Work includes construction, maintenance during construction, warranty as provided in the Contract, and other incidental work.

The Contractor shall be responsible for furnishing all supervision, labor, equipment, tools supplies, permanent materials and temporary materials required to perform the Work including construction quality control including inspection, testing and documentation, all required documentation at the conclusion of the project, warranting its work and performing all other work indicated in the Contract.

The Department shall have the right to alter the nature and extent of the Work as provided in the Contract; payment to be made as provided in the same.

B. Time.

The Contractor agrees to complete all Work, except warranty work, on or before **October 30, 2004.** Further, the Department may deduct from moneys otherwise due the Contractor, not as a penalty, but as Liquidated Damages in accordance with Sections 107.7 and 107.8 of the State of Maine Department of Transportation Standard Specifications, Revision of December 2002.

C. Price.

The quantities given in the Schedule of Items of the Bid Package will be used as the basis for determining the original Contract amount and for determining the amounts of the required Performance Surety Bond and Payment Surety Bond, and that the amount of this offer is _____

\$_____ Performance Bond and Payment Bond each being 100% of the amount of this Contract.

D. Contract.

This Contract, which may be amended, modified, or supplemented in writing only, consists of the Contract documents as defined in the Plans, Standard Specifications, Revision of December 2002, Standard Details Revision of December 2002 as updated through advertisement, Supplemental Specifications, Special Provisions, Contract Agreement; and Contract Bonds. It is agreed and understood that this Contract will be governed by the documents listed above.

E. Certifications.

By signing below, the Contractor hereby certifies that to the best of the Contractor's knowledge and belief:

1. All of the statements, representations, covenants, and/or certifications required or set forth in the Bid and the Bid Documents, including those in Appendix A to Division 100 of the Standard Specifications Revision of December 2002 (Federal Contract Provisions Supplement), and the Contract are still complete and accurate as of the date of this Agreement.
2. The Contractor knows of no legal, contractual, or financial impediment to entering into this Contract.
3. The person signing below is legally authorized by the Contractor to sign this Contract on behalf of the Contractor and to legally bind the Contractor to the terms of the Contract.

F. Offer.

The undersigned, having carefully examined the site of work, the Plans, Standard Specifications Revision of December 2002, Standard Details Revision of December 2002 as updated through advertisement, Supplemental Specifications, Special Provisions, Contract Agreement; and Contract Bonds contained herein for construction of: **STP-1020(700)X, PIN 10207.00, for the Hot Mix Asphalt Overlay, Plant Mix Recycled Asphalt Pavement, Variable Depth Gravel Areas, Full Construction Areas, Drainage and Safety Improvements in the town of Alton, County of Penobscot, Maine** on which bids will be received until the time specified in the “Notice to Contractors” do(es) hereby bid and offer to enter into this contract to supply all the materials, tools, equipment and labor to construct the whole of the Work in strict accordance with the terms and conditions of this Contract at the unit prices in the attached “Schedule of Items”.

The Offeror agrees to perform the work required at the price specified above and in accordance with the bids provided in the attached “Schedule of Items” in strict accordance with the terms of this solicitation, and to provide the appropriate insurance and bonds if this offer is accepted by the Government in writing.

As Offeror also agrees:

First: To do any extra work, not covered by the attached “Schedule of Items”, which may be ordered by the Resident, and to accept as full compensation the amount determined upon a “Force Account” basis as provided in the Standard Specifications, Revision of December 2002, and as addressed in the contract documents.

Second: That the bid bond at 5% of the bid amount or the official bank check, cashier’s check, certificate of deposit or U. S. Postal Money Order in the amount given in the “Notice to Contractors”, payable to the Treasurer of the State of Maine and accompanying this bid, shall be forfeited, as liquidated damages, if in case this bid is accepted, and the undersigned shall fail to abide by the terms and conditions of the offer and fail to furnish satisfactory insurance and Contract bonds under the conditions stipulated in the Specifications within 15 days of notice of intent to award the contract.

Third: To begin the Work on the date specified in the Engineer’s “Notice to Commence Work” as stated in Section 107.2 of the Standard Specifications Revision of December 2002 and complete the Work within the time limits given in the Special Provisions of this Contract.

Fourth: The Contractor will be bound to the Disadvantaged Business Enterprise (DBE) Requirements contained in the attached Notice (Additional Instructions to Bidders) and submit a completed Contractor’s Disadvantaged Business Enterprise Utilization Plan by 4:30pm on the day of bid opening to the Contracts Engineer.

Fifth: That this offer shall remain open for 30 calendar days after the date of opening of bids.

Sixth: The Bidder hereby certifies, to the best of its knowledge and belief that: the Bidder has not, either directly or indirectly, entered into any agreement, participated in any collusion, or otherwise taken any action in restraint of competitive bidding in connection with its bid, and its subsequent contract with the Department.

IN WITNESS WHEREOF, the Contractor, for itself, its successors and assigns, hereby execute two duplicate originals of this Agreement and thereby binds itself to all covenants, terms, and obligations contained in the Contract Documents.

CONTRACTOR

Date

(Signature of Legally Authorized Representative
of the Contractor)

Witness

(Name and Title Printed)

G. Award.

Your offer is hereby accepted.
documents referenced herein.

This award consummates the Contract, and the

MAINE DEPARTMENT OF TRANSPORTATION

Date

By: David A. Cole, Commissioner

Witness

CONTRACT AGREEMENT, OFFER & AWARD

AGREEMENT made on the date last signed below, by and between the State of Maine, acting through and by its Department of Transportation (Department), an agency of state government with its principal administrative offices located at 1705 U.S. Route 202, Winthrop, Maine, with a mailing address at 16 State House Station, Augusta, Maine 04333-0016, and (Name of the firm bidding the job) a corporation or other legal entity organized under the laws of the State of Maine, with its principal place of business located at (address of the firm bidding the job)

The Department and the Contractor, in consideration of the mutual promises set forth in this Agreement (the "Contract"), hereby agree as follows:

A. The Work.

The Contractor agrees to complete all Work as specified or indicated in the Contract including Extra Work in conformity with the Contract, PIN No. 1224.00

for the Hot Mix Asphalt Overlay in the town/city of West Eastport, County of Washington, Maine. The Work includes construction, maintenance during construction, warranty as provided in the Contract, and other incidental work.

The Contractor shall be responsible for furnishing all supervision, labor, equipment, tools supplies, permanent materials and temporary materials required to perform the Work including construction quality control including inspection, testing and documentation, all required documentation at the conclusion of the project, warranting its work and performing all other work indicated in the Contract.

The Department shall have the right to alter the nature and extent of the Work as provided in the Contract; payment to be made as provided in the same.

B. Time.

The Contractor agrees to complete all Work, except warranty work, on or before November 15, 2003. Further, the Department may deduct from moneys otherwise due the Contractor, not as a penalty, but as Liquidated Damages in accordance with Sections 107.7 and 107.8 of the State of Maine Department of Transportation Standard Specifications, Revision of December 2002.

C. Price.

The quantities given in the Schedule of Items of the Bid Package will be used as the basis for determining the original Contract amount and for determining the amounts of the required Performance Surety Bond and Payment Surety Bond, and that the amount of this offer is (Place bid here in alphabetical form such as One Hundred and Two dollars and 10 cents)
\$ (repeat bid here in numerical terms, such as \$102.10) Performance Bond and Payment Bond each being 100% of the amount of this Contract.

D. Contract.

This Contract, which may be amended, modified, or supplemented in writing only, consists of the Contract documents as defined in the Plans, Standard Specifications, Revision of December 2002, Standard Details Revision of December 2002, Supplemental Specifications, Special Provisions, Contract Agreement; and Contract Bonds. It is agreed and understood that this Contract will be governed by the documents listed above.

E. Certifications.

By signing below, the Contractor hereby certifies that to the best of the Contractor's knowledge and belief:

1. All of the statements, representations, covenants, and/or certifications required or set forth in the Bid and the Bid Documents, including those in Appendix A to Division 100 of the Standard Specifications Revision of December 2002 (Federal Contract Provisions Supplement), and the Contract are still complete and accurate as of the date of this Agreement.
2. The Contractor knows of no legal, contractual, or financial impediment to entering into this Contract.
3. The person signing below is legally authorized by the Contractor to sign this Contract on behalf of the Contractor and to legally bind the Contractor to the terms of the Contract.

F. Offer.

The undersigned, having carefully examined the site of work, the Plans, Standard Specifications, Revision of December 2002, Standard Details Revision of December 2002, Supplemental Specifications, Special Provisions, Contract Agreement; and Contract Bonds contained herein for construction of:

PIN 1234.00 West Eastport, Hot Mix Asphalt Overlay

State of Maine, on which bids will be received until the time specified in the "Notice to Contractors" do(es) hereby bid and offer to enter into this contract to supply all the materials, tools, equipment and labor to construct the whole of the Work in strict accordance with the terms and conditions of this Contract at the unit prices in the attached "Schedule of Items".

The Offeror agrees to perform the work required at the price specified above and in accordance with the bids provided in the attached "Schedule of Items" in strict accordance with the terms of this solicitation, and to provide the appropriate insurance and bonds if this offer is accepted by the Government in writing.

As Offeror also agrees:

First: To do any extra work, not covered by the attached "Schedule of Items", which may be ordered by the Resident, and to accept as full compensation the amount determined upon a "Force Account" basis as provided in the Standard Specifications, Revision of December 2002, and as addressed in the contract documents.

Second: That the bid bond at 5% of the bid amount or the official bank check, cashier's check, certificate of deposit or U. S. Postal Money Order in the amount given in the "Notice to Contractors", payable to the Treasurer of the State of Maine and accompanying this bid, shall be forfeited, as liquidated damages, if in case this bid is accepted, and the undersigned shall fail to abide by the terms and conditions of the offer and fail to furnish satisfactory insurance and Contract bonds under the conditions stipulated in the Specifications within 15 days of notice of intent to award the contract.

Third: To begin the Work on the date specified in the Engineer's "Notice to Commence Work" as stated in Section 107.2 of the Standard Specifications Revision of 2002 and complete the Work within the time limits given in the Special Provisions of this Contract.

Fourth: The Contractor will be bound to the Disadvantaged Business Enterprise (DBE) Requirements contained in the attached Notice (Additional Instructions to Bidders) and submit a completed Contractor's Disadvantaged Business Enterprise Utilization Plan by 4:30pm on the day of bid opening to the Contracts Engineer.

Fifth: That this offer shall remain open for 30 calendar days after the date of opening of bids.

Sixth: The Bidder hereby certifies, to the best of its knowledge and belief that: the Bidder has not, either directly or indirectly, entered into any agreement, participated in any collusion, or otherwise taken any action in restraint of competitive bidding in connection with its bid, and its subsequent contract with the Department.

IN WITNESS WHEREOF, the Contractor, for itself, its successors and assigns, hereby execute two duplicate originals of this Agreement and thereby binds itself to all covenants, terms, and obligations contained in the Contract Documents.

Date

(Witness Sign Here)
Witness

CONTRACTOR
(Sign Here)

(Signature of Legally Authorized Representative
of the Contractor)

(Print Name Here)
(Name and Title Printed)

G. Award.

Your offer is hereby accepted.
documents referenced herein.

This award consummates the Contract, and the

MAINE DEPARTMENT OF TRANSPORTATION

Date

By: David A. Cole, Commissioner

(Witness)

BOND # _____

CONTRACT PERFORMANCE BOND
(Surety Company Form)

KNOW ALL MEN BY THESE PRESENTS: That _____
_____ **and the State of** _____, as principal,
and _____,
a corporation duly organized under the laws of the State of _____ and having a
usual place of business _____,
as Surety, are held and firmly bound unto the Treasurer of the State of Maine in the sum
of _____ **and 00/100 Dollars (\$** _____ **)**,
to be paid said Treasurer of the State of Maine or his successors in office, for which
payment well and truly to be made, Principal and Surety bind themselves, their heirs,
executors and administrators, successors and assigns, jointly and severally by these
presents.

The condition of this obligation is such that if the Principal designated as Contractor in
the Contract to construct Project Number _____ in the Municipality of _____
promptly and faithfully performs the Contract, then this
obligation shall be null and void; otherwise it shall remain in full force and effect.

The Surety hereby waives notice of any alteration or extension of time made by the State
of Maine.

Signed and sealed this _____ day of _____, 20_____.

WITNESSES:

Signature.....
Print Name Legibly

Signature

Print Name Legibly

SURETY ADDRESS:

.....
.....
.....

TELEPHONE.....

SIGNATURES:

CONTRACTOR:

.....
Print Name Legibly

SURETY:

.....
Print Name Legibly

NAME OF LOCAL AGENCY:

ADDRESS

.....
.....

.....

BOND # _____

CONTRACT PAYMENT BOND
(Surety Company Form)

KNOW ALL MEN BY THESE PRESENTS: That _____
_____ **and the State of** _____, as principal,
and _____
a corporation duly organized under the laws of the State of _____ and having a
usual place of business in _____,
as Surety, are held and firmly bound unto the Treasurer of the State of Maine for the use
and benefit of claimants as herein below defined, in the sum of
_____ **and 00/100 Dollars (\$** _____ **)**
for the payment whereof Principal and Surety bind themselves, their heirs, executors and
administrators, successors and assigns, jointly and severally by these presents.

The condition of this obligation is such that if the Principal designated as Contractor in
the Contract to construct Project Number _____ in the Municipality of
_____ promptly satisfies all claims and demands incurred for all
labor and material, used or required by him in connection with the work contemplated by
said Contract, and fully reimburses the obligee for all outlay and expense which the
obligee may incur in making good any default of said Principal, then this obligation shall
be null and void; otherwise it shall remain in full force and effect.

A claimant is defined as one having a direct contract with the Principal or with a
Subcontractor of the Principal for labor, material or both, used or reasonably required for
use in the performance of the contract.

Signed and sealed this _____ day of _____, 20 .. .

WITNESS:

SIGNATURES:

CONTRACTOR:

Signature.....

Print Name Legibly

SURETY:

Signature.....

Print Name Legibly

SURETY ADDRESS:

NAME OF LOCAL AGENCY:

ADDRESS

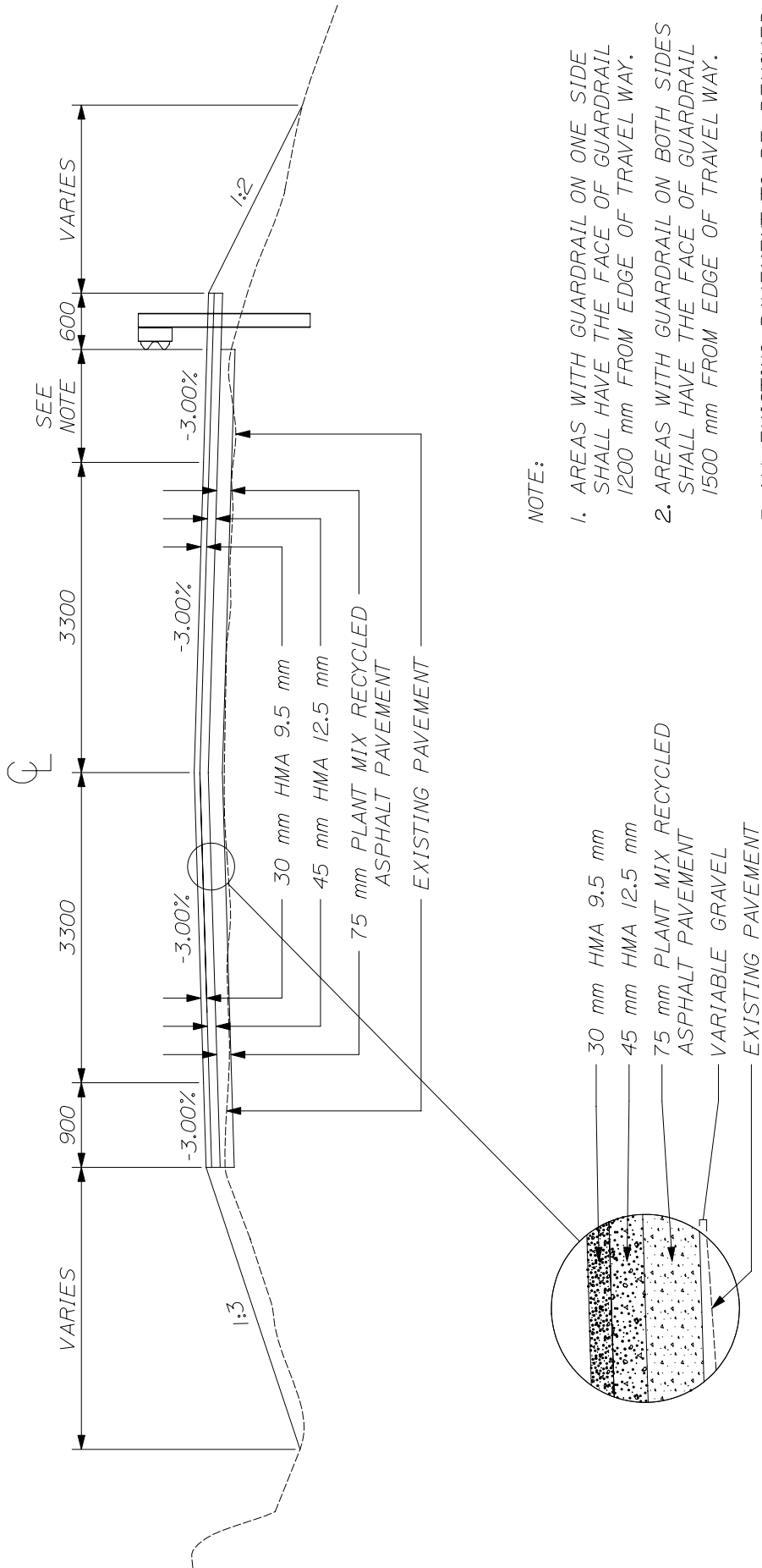
TELEPHONE

SPECIAL PROVISION PARTNERING

The successful bidder will have the opportunity to enter into a cooperative partnership agreement with the State Department of Transportation for the contract. The objective of this agreement is the effective completion of the work on time and to the standard of quality that will be a source of pride to both the State and the Contractor. The partnering agreement will not affect the terms of the contract. It is intended only to establish an environment of cooperation between the parties. If the partnering agreement is accepted.

1. Contractor shall select and provide a third-party facilitator to conduct the team building workshop for the Contractor and Department personnel. Facilitator selection shall require Department concurrence. The cost for the facilitator and his associated expenses will be shared equally by the Department on the next monthly estimate, following receipt of invoice(s) from the Contractor, on an extra work basis.
2. Contractor and Department will exchange lists of the key personnel to be participants in the workshop. The list will contain the name and job title of each person, a contact phone number, and the address for job related correspondence.
3. The Contractor shall select the location and make all arrangements for space as required by facilitator, and for any meals required. This cost to be shared equally.
4. A working arrangement for the partnership will be agreed upon in writing at the workshop. The arrangement will set out the mutually recognized goals and expectation of the parties.
5. The Contractor and the Department agree to make an effort to maintain identified key personnel assigned to the work for its duration. A timely notice by each shall be given if changes by either must be made.
6. Project issues shall be processed in the manner agreed upon by the parties during the orientation.
7. Follow-up workshops may be held periodically throughout the duration of the contract as agreed by the Contractor and the Department.
8. The Partnering Agreement is not intended to be a legal document. Failure by either party to follow the process identified will not be grounds for any claim under the contract.
9. ARE YOU INTERESTED IN THIS OPPORTUNITY? YES _____ NO _____

VARIABLE GRAVEL - NORMAL
75 mm HOT MIX ASPHALT OVERLAY WITH
75 mm PLANT MIX RECYCLED ASPHALT PAVEMENT



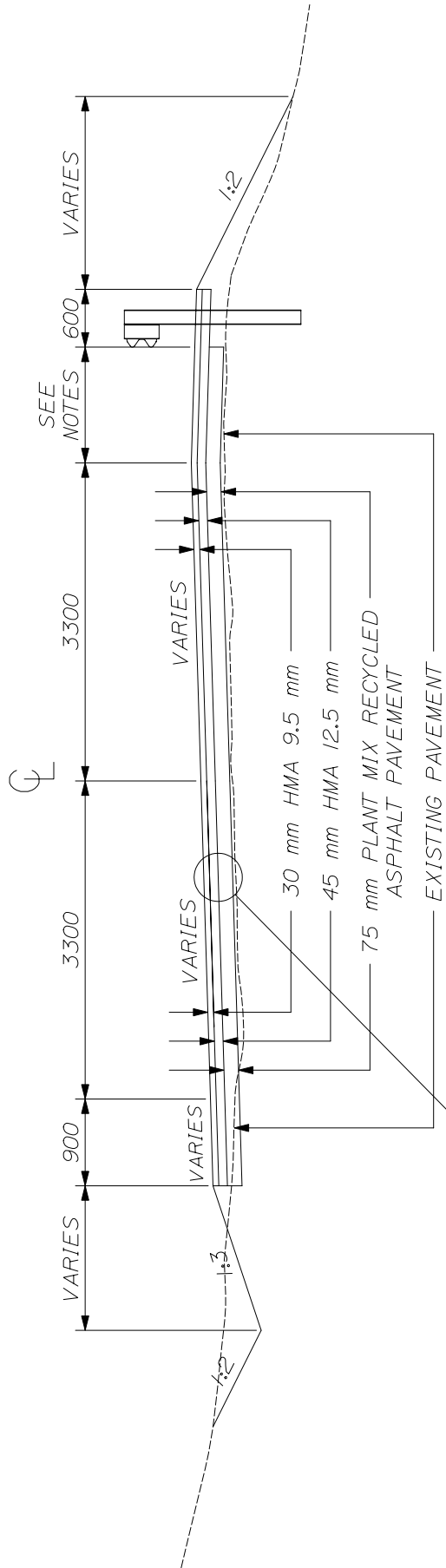
NOTE:

1. AREAS WITH GUARDRAIL ON ONE SIDE SHALL HAVE THE FACE OF GUARDRAIL 1200 mm FROM EDGE OF TRAVEL WAY.
2. AREAS WITH GUARDRAIL ON BOTH SIDES SHALL HAVE THE FACE OF GUARDRAIL 1500 mm FROM EDGE OF TRAVEL WAY.
3. ALL EXISTING PAVEMENT TO BE REMOVED AND INCORPORATED INTO THE RECYCLED ASPHALT PAVEMENT.

NOTE: NOT TO SCALE

STATE OF MAINE DEPARTMENT OF TRANSPORTATION	ROUTE 16 ALTON PENOBSCOT COUNTY	SHEET NUMBER 1
10207.00	TYPICALS	OF 6

VARIABLE GRAVEL - SUPERELEVATED
75 mm HOT MIX ASPHALT OVERLAY WITH
75 mm PLANT MIX RECYCLED ASPHALT PAVEMENT

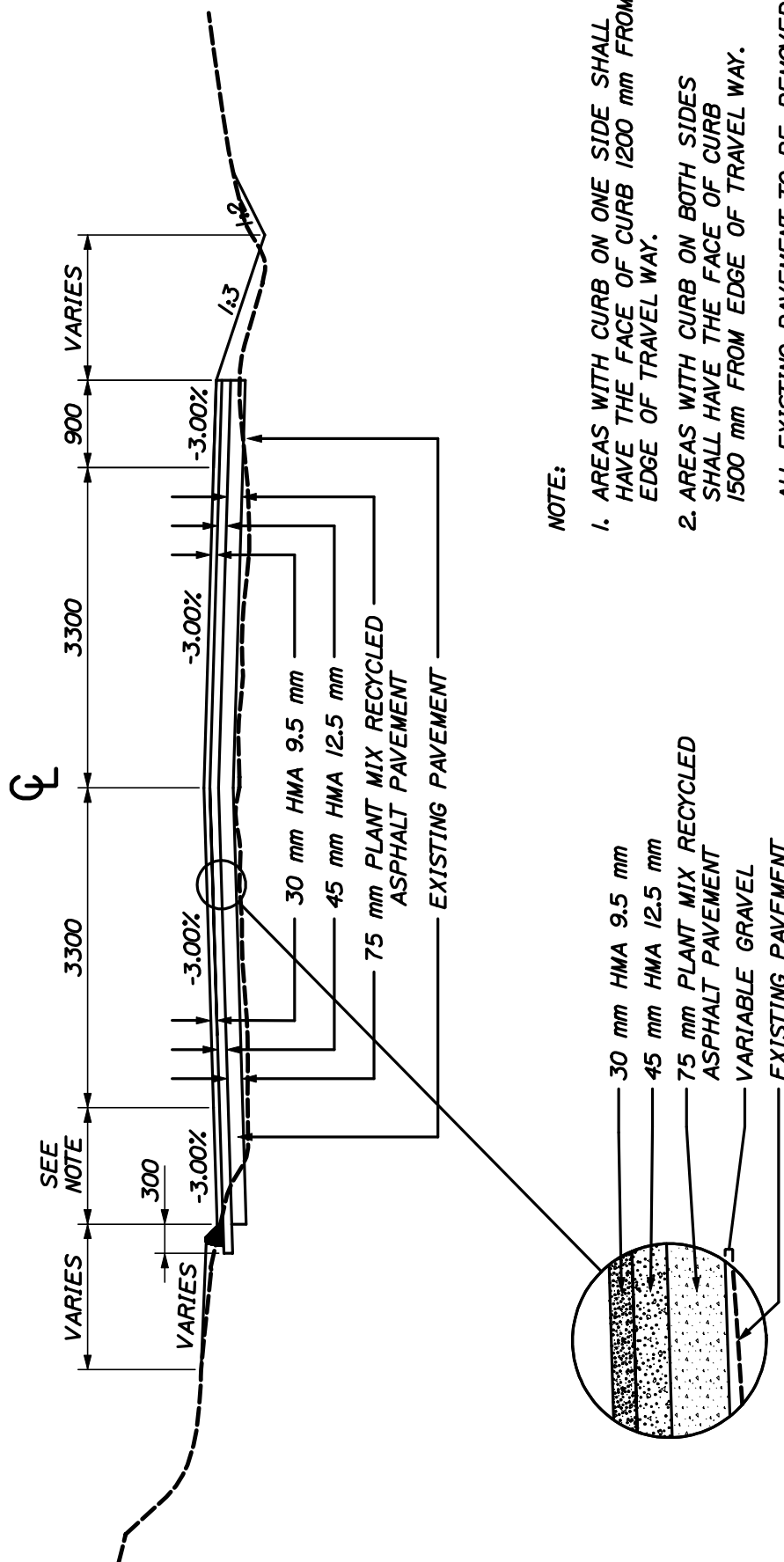


NOTES:

1. AREAS WITH GUARDRAIL ON ONE SIDE SHALL HAVE THE FACE OF GUARDRAIL 1200 mm FROM EDGE OF TRAVEL WAY.
2. AREAS WITH GUARDRAIL ON BOTH SIDES SHALL HAVE THE FACE OF GUARDRAIL 1500 mm FROM EDGE OF TRAVEL WAY.
3. ON HIGH SIDE OF SUPERELEVATED CURVES 900 mm SHOULDERS WILL BE GRADED TO MATCH THE TRAVEL WAY.
4. ON HIGH SIDE OF SUPERELEVATED CURVES 1200 mm OR WIDER SHOULDERS WILL BREAK AS SHOWN.
5. SHOULDER CROSS SLOPE SHALL BE -2% ON THE HIGH SIDE OF A SUPER-ELEVATION WHEN THE ROLLOVER ALGEBRAIC DIFFERENCE IN THE RATE OF CROSS SLOPE EXCEEDS 8%.
6. ALL EXISTING PAVEMENT TO BE REMOVED AND INCORPORATED INTO THE RECYCLED ASPHALT PAVEMENT.

NOTE: NOT TO SCALE

VARIABLE GRAVEL - NORMAL
75 mm HOT MIX ASPHALT OVERLAY WITH
75 mm PLANT MIX RECYCLED ASPHALT PAVEMENT



NOTE:

1. AREAS WITH CURB ON ONE SIDE SHALL HAVE THE FACE OF CURB 1200 mm FROM EDGE OF TRAVEL WAY.
2. AREAS WITH CURB ON BOTH SIDES SHALL HAVE THE FACE OF CURB 1500 mm FROM EDGE OF TRAVEL WAY.
3. ALL EXISTING PAVEMENT TO BE REMOVED AND INCORPORATED INTO THE RECYCLED ASPHALT PAVEMENT

NOTE: NOT TO SCALE

STATE OF MAINE
DEPARTMENT OF TRANSPORTATION

ROUTE 16

ALTON

SHEET NUMBER

PENOBSCOT COUNTY

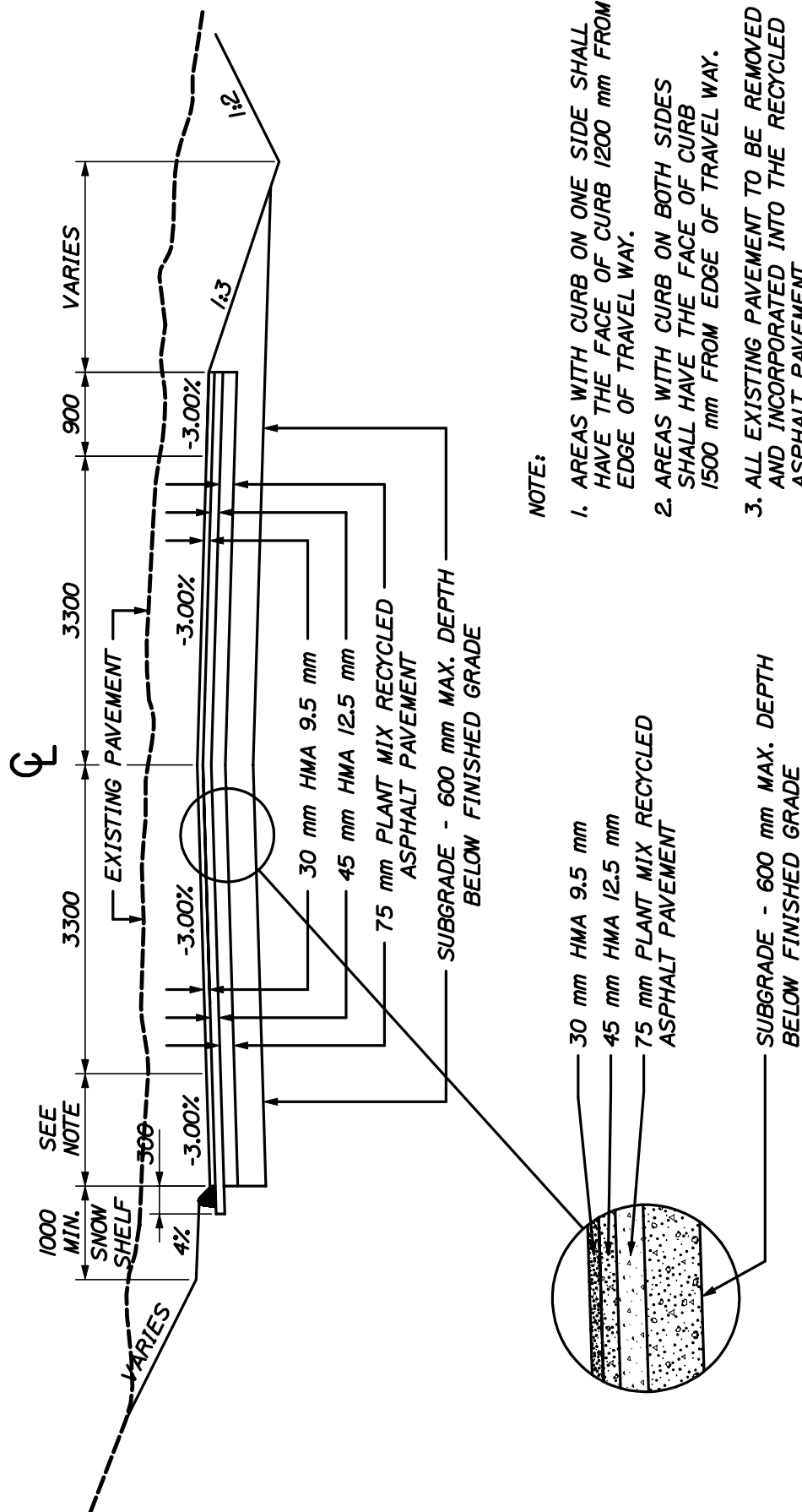
3

10207.00

TYPICALS

OF 6

FULL RECONSTRUCTION - NORMAL - CUT AREA
 75 mm HOT MIX ASPHALT OVERLAY WITH
 75 mm PLANT MIX RECYCLED ASPHALT PAVEMENT



NOTE:

1. AREAS WITH CURB ON ONE SIDE SHALL HAVE THE FACE OF CURB 1200 mm FROM EDGE OF TRAVEL WAY.
2. AREAS WITH CURB ON BOTH SIDES SHALL HAVE THE FACE OF CURB 1500 mm FROM EDGE OF TRAVEL WAY.
3. ALL EXISTING PAVEMENT TO BE REMOVED AND INCORPORATED INTO THE RECYCLED ASPHALT PAVEMENT.

NOTE: NOT TO SCALE

STATE OF MAINE
 DEPARTMENT OF TRANSPORTATION

ROUTE 16

ALTON

SHEET NUMBER

PENOBSCOT COUNTY

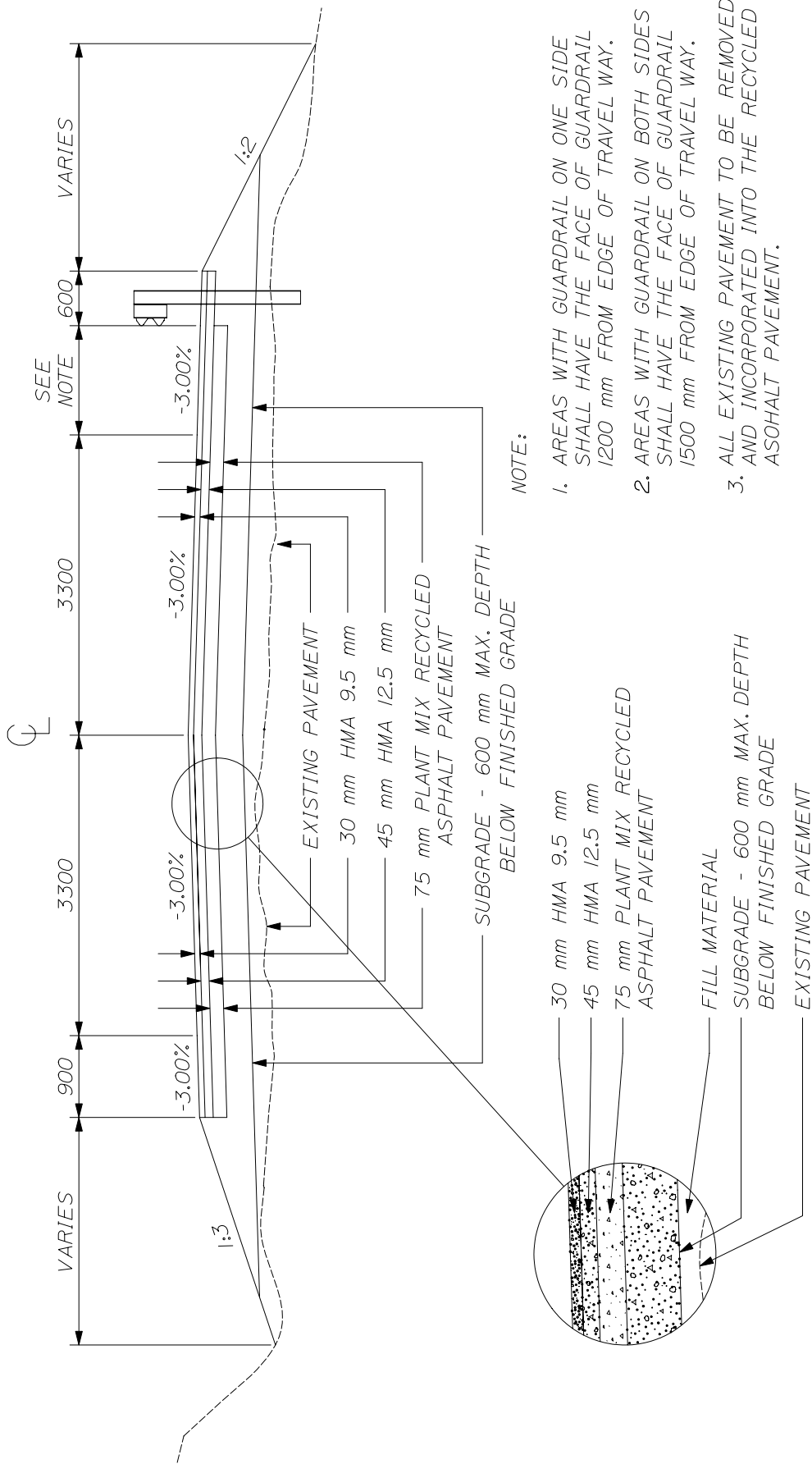
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10207.00

TYPICALS

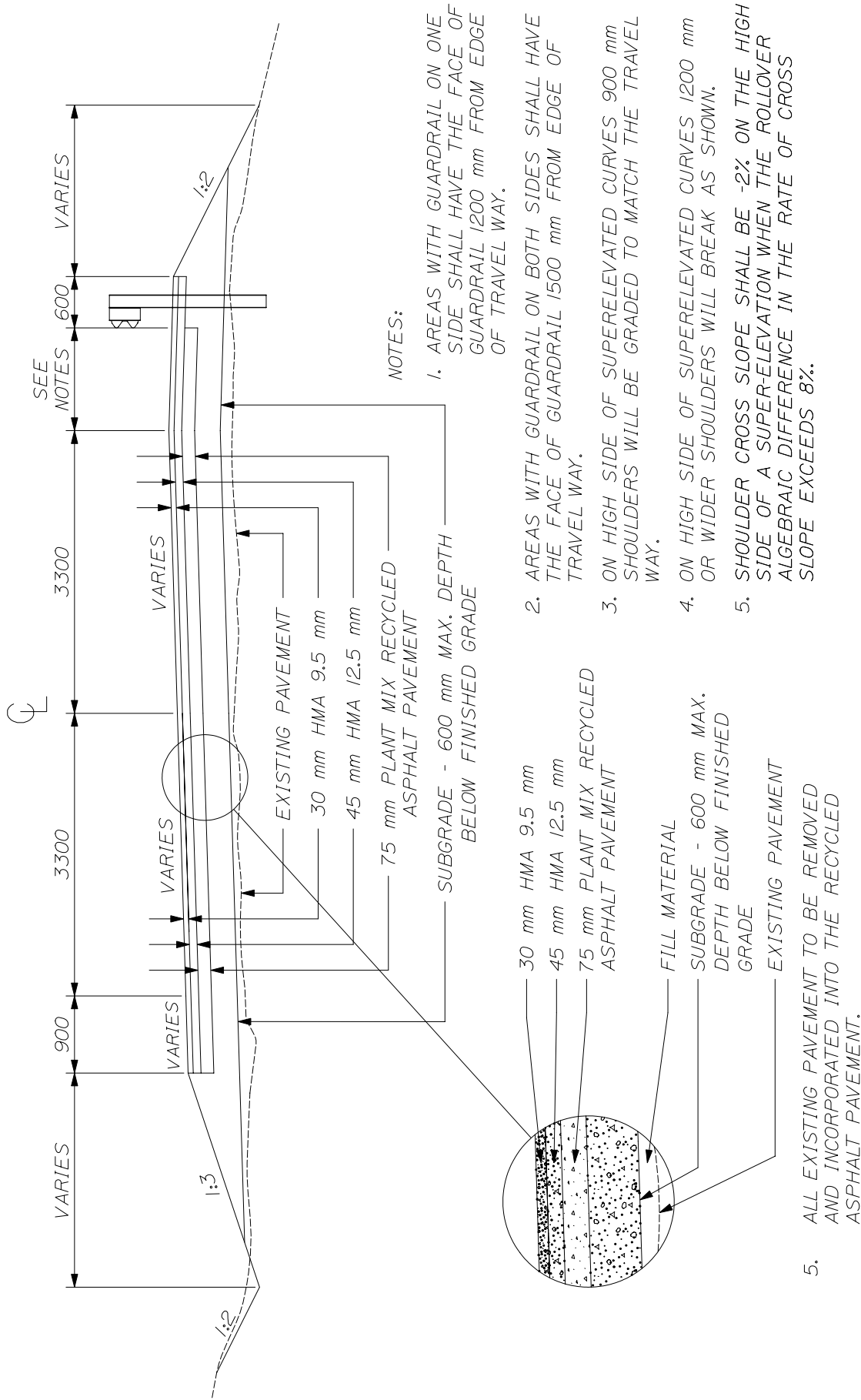
OF 6

FULL RECONSTRUCTION - NORMAL - FILL AREA
75 mm HOT MIX ASPHALT OVERLAY WITH
75 mm PLANT MIX RECYCLED ASPHALT PAVEMENT



NOTE: NOT TO SCALE

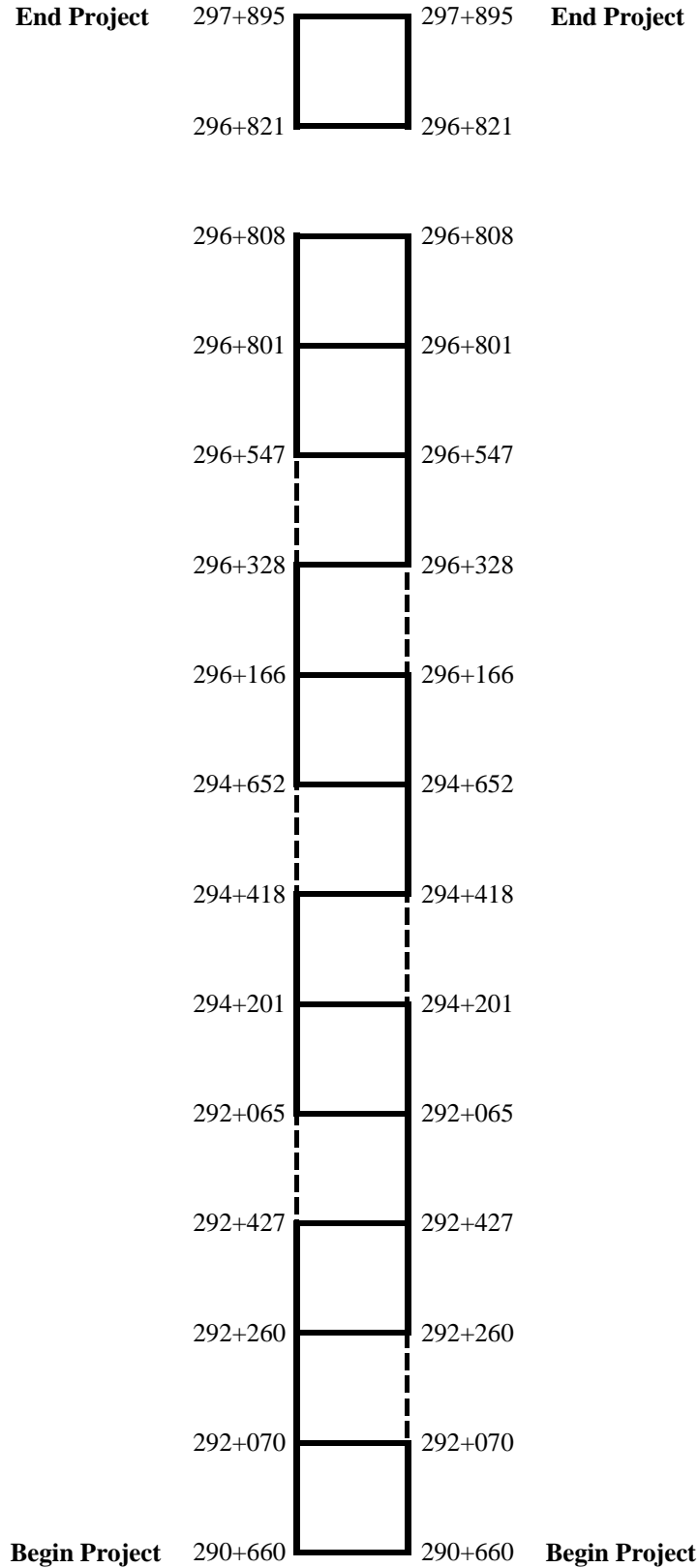
FULL RECONSTRUCTION - SUPERELEVATED
75 mm HOT MIX ASPHALT OVERLAY WITH
75 mm PLANT MIX RECYCLED ASPHALT PAVEMENT



ALTON
PIN 10207.00
ROUTE 16 CHIP
PROJECT STATIONING

Lt	Station	Rt
End Project	297+895	End Project
Argyle Road	296+812	
	294+857	Pickerel Pond Rd
	294+100	Bradford Station Rd
Cable route sign 11-1 3f	293+168	
Pole # 366	⊖ 291+226	
Tenmile Brook	290+788	Tenmile Brook
Begin Project	290+660	Begin Project

ALTON
PIN 10207.00
ROUTE 16 CHIP
PROJECT STRIPING



ALTON
PIN 10207.00
ROUTE 16 CHIP

PROJECT SUPERELEVATIONS

match	290+660	match
-4.44%	290+680	-4.24%
-3.42%	290+700	-0.92%
-2.40%	290+720	2.40%
-4.00%	290+740	4.00%
"	" "	"
-4.00%	290+800	4.00%
-3.00%	290+820	1.70%
-3.00%	290+840	-0.60%
-3.00%	290+860	-3.00%
-3.00%	290+880	-3.00%
-0.80%	290+900	-3.00%
1.40%	290+920	-3.00%
3.60%	290+940	-4.40%
5.80%	290+960	-5.80%
"	" "	"
5.80%	291+120	-5.80%
3.60%	291+140	-4.40%
1.40%	291+160	-3.00%
-0.80%	291+180	-3.00%
-3.00%	291+200	-3.00%
-3.00%	291+200	-3.00%
-3.00%	291+220	-0.90%
-3.00%	291+240	1.30%
-4.30%	291+260	3.40%
-5.60%	291+280	5.60%
"	" "	"
-5.60%	291+360	5.60%
-4.30%	291+380	3.40%
-3.00%	291+400	1.30%
-3.00%	291+420	-0.90%
-3.00%	291+440	-3.00%
"	" "	"

ALTON
PIN 10207.00
ROUTE 16 CHIP

PROJECT SUPERELEVATIONS

"	" "	"
-3.00%	291+520	-3.00%
-1.00%	291+540	-3.00%
1.00%	291+560	-3.00%
3.00%	291+580	-3.00%
5.00%	291+600	-5.00%
"	" "	"
5.00%	291+740	-5.00%
2.80%	291+760	-2.80%
0.60%	291+780	-0.60%
-1.60%	291+800	1.60%
-3.80%	291+820	3.80%
-6.00%	291+840	6.00%
"	" "	"
-6.00%	291+920	6.00%
-3.50%	291+940	3.50%
-3.00%	291+960	1.00%
-3.00%	291+980	-1.50%
-3.00%	292+000	0.00%
-3.00%	292+020	1.50%
-3.10%	292+040	3.10%
"	" "	"
-3.10%	292+140	3.10%
-3.00%	292+160	1.10%
-3.00%	292+180	-0.90%
-3.00%	292+200	-3.00%
"	" "	"
"	" "	"
-3.00%	292+580	-3.00%
-1.20%	292+600	-3.00%
0.50%	292+620	-3.00%
2.20%	292+640	-3.00%
4.00%	292+660	-4.00%
"	" "	"
4.00%	292+720	-4.00%
2.20%	292+740	-3.00%
0.50%	292+760	-3.00%
-1.20%	292+780	-3.00%
-3.00%	292+800	-3.00%
"	" "	"

ALTON
PIN 10207.00
ROUTE 16 CHIP

PROJECT SUPERELEVATIONS

"	" "	"
-3.00%	293+020	-3.00%
-0.70%	293+040	-3.00%
1.50%	293+060	-3.00%
3.80%	293+080	-4.50%
6.00%	293+100	-6.00%
6.00%	293+120	-6.00%
6.00%	293+140	-6.00%
4.00%	293+160	-4.00%
2.00%	293+180	-2.00%
0.00%	293+200	0.00%
-2.00%	293+220	2.00%
-4.00%	293+240	4.00%
-6.00%	293+260	6.00%
-6.00%	293+280	6.00%
-4.50%	293+300	3.80%
-3.00%	293+320	1.50%
-3.00%	293+340	-0.70%
-3.00%	293+360	-3.00%
"	" "	"
"	" "	"
-3.00%	293+480	-3.00%
-3.00%	293+500	-1.20%
-3.00%	293+520	0.60%
-3.00%	293+540	2.30%
-4.00%	293+560	4.00%
"	" "	"
-4.00%	293+640	4.00%
-1.50%	293+660	1.50%
1.00%	293+680	-1.00%
3.50%	293+700	-3.50%
6.00%	293+720	-6.00%
"	" "	"
6.00%	293+840	-6.00%
4.00%	293+860	-4.00%
2.00%	293+880	-2.00%
0.00%	293+900	0.00%
-1.90%	293+920	1.90%
-3.80%	293+940	3.80%
-5.80%	293+960	5.80%
"	" "	"
-5.80%	294+260	5.80%
-4.40%	294+280	3.60%
-3.00%	294+300	1.40%
-3.00%	294+320	-0.80%
-3.00%	294+340	-3.00%
"	" "	"

ALTON
PIN 10207.00
ROUTE 16 CHIP

PROJECT SUPERELEVATIONS

"	" "	"
-3.00%	294+660	-3.00%
-1.00%	294+680	-3.00%
0.90%	294+700	-3.00%
2.90%	294+720	-3.00%
4.80%	294+740	-4.80%
"	" "	"
4.80%	294+900	-4.80%
2.90%	294+920	-3.00%
0.90%	294+940	-3.00%
-1.00%	294+960	-3.00%
-3.00%	294+980	-3.00%
"	" "	"
"	" "	"
-3.00%	295+040	-3.00%
-1.00%	295+060	-3.00%
0.90%	295+080	-3.00%
2.90%	295+100	-3.00%
4.80%	295+120	-4.80%
"	" "	"
4.80%	295+300	-4.80%
2.90%	295+320	-3.00%
0.90%	295+340	-3.00%
-1.00%	295+360	-3.00%
-3.00%	295+380	-3.00%
"	" "	"
"	" "	"
-3.00%	295+460	-3.00%
-1.20%	295+480	-3.00%
0.60%	295+500	-3.00%
2.40%	295+520	-3.00%
4.30%	295+540	-4.30%
"	" "	"
4.30%	295+620	-4.30%
2.20%	295+640	-2.20%
0.10%	295+660	-0.10%
-2.00%	295+680	2.00%
-4.00%	295+700	4.00%
-6.00%	295+720	6.00%
"	" "	"
-6.00%	295+800	6.00%
-4.50%	295+820	3.70%
-3.00%	295+840	1.50%
-3.00%	295+860	-0.70%
-3.00%	295+880	-3.00%
"	" "	"

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"	" "	"
-3.00%	295+960	-3.00%
-0.80%	295+980	-3.00%
1.60%	296+000	-3.00%
3.80%	296+020	-3.80%
6.00%	296+040	-6.00%
"	" "	"
6.00%	296+120	-6.00%
3.80%	296+140	-3.80%
1.60%	296+160	-3.00%
-0.80%	296+180	-3.00%
-3.00%	296+200	-3.00%
"	" "	"
"	" "	"
-3.00%	297+100	-3.00%
-0.80%	297+120	-3.00%
1.40%	297+140	-3.00%
3.60%	297+160	-4.40%
5.70%	297+180	-5.70%
"	" "	"
5.70%	297+280	-5.70%
3.60%	297+300	-4.40%
1.40%	297+320	-3.00%
-0.80%	297+340	-3.00%
-3.00%	297+360	-3.00%
"	" "	"
"	" "	"
-3.00%	297+460	-3.00%
-3.00%	297+480	-1.30%
-3.00%	297+500	0.50%
-3.00%	297+520	2.20%
"	" "	"
-3.00%	297+620	2.20%
-3.00%	297+640	0.50%
-3.00%	297+660	-1.30%
-3.00%	297+680	-3.00%

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Station	Existing Elevation	Markup/Down mm	Proposed Elevation	Station	Existing Elevation	Markup/Down mm	Proposed Elevation
290+660	51.241	0	51.241	291+160	53.407	62	53.469
290+670	51.166	0	51.166	291+170	53.334	75	53.409
290+680	50.990	0	50.990	291+180	53.235	101	53.336
290+690	50.790	0	50.790	291+190	53.145	75	53.220
290+700	50.467	20	50.487	291+200	53.062	50	53.112
290+710	50.154	54	50.208	291+210	52.995	25	53.020
290+720	49.861	56	49.917	291+220	52.930	0	52.930
290+730	49.573	53	49.626	291+230	52.903	0	52.903
290+740	49.322	38	49.360	291+240	52.909	0	52.909
290+750	49.117	10	49.127	291+250	52.998	0	52.998
290+760	48.908	20	48.928	291+260	53.096	0	53.096
290+770	48.760	3	48.763	291+270	53.215	0	53.215
290+780	48.614	18	48.632	291+280	53.352	0	53.352
290+790	48.517	18	48.535	291+290	53.476	0	53.476
290+800	48.445	26	48.471	291+300	53.613	0	53.613
290+810	48.443	23	48.466	291+310	53.760	0	53.760
290+820	48.475	20	48.495	291+320	53.899	0	53.899
290+830	48.565	-7	48.558	291+330	54.045	0	54.045
290+840	48.675	-21	48.654	291+340	54.191	0	54.191
290+850	48.804	-20	48.784	291+350	54.322	0	54.322
290+860	48.938	1	48.939	291+360	54.457	0	54.457
290+870	49.075	19	49.094	291+370	54.560	0	54.560
290+880	49.228	47	49.275	291+380	54.651	0	54.651
290+890	49.384	72	49.456	291+390	54.737	0	54.737
290+900	49.554	82	49.636	291+400	54.834	0	54.834
290+910	49.725	92	49.817	291+410	54.865	0	54.865
290+920	49.898	99	49.997	291+420	54.885	0	54.885
290+930	50.084	94	50.178	291+430	54.902	0	54.902
290+940	50.292	66	50.358	291+440	54.879	0	54.879
290+950	50.493	45	50.538	291+450	54.854	0	54.854
290+960	50.689	11	50.700	291+460	54.834	0	54.834
290+970	50.773	64	50.837	291+470	54.800	0	54.800
290+980	50.821	102	50.923	291+480	54.766	0	54.766
290+990	50.806	179	50.985	291+490	54.706	0	54.706
291+000	50.806	223	51.029	291+500	54.646	0	54.646
291+010	50.878	193	51.071	291+510	54.578	0	54.578
291+020	50.953	163	51.116	291+520	54.508	0	54.508
291+030	51.027	171	51.198	291+530	54.492	0	54.492
291+040	51.155	177	51.332	291+540	54.480	0	54.480
291+050	51.337	182	51.519	291+550	54.460	0	54.460
291+060	51.619	138	51.757	291+560	54.432	0	54.432
291+070	51.920	113	52.033	291+570	54.386	0	54.386
291+080	52.259	52	52.311	291+580	54.348	0	54.348
291+090	52.597	-8	52.589	291+590	54.325	0	54.325
291+100	52.870	-16	52.854	291+600	54.274	0	54.274
291+110	53.134	-61	53.073	291+610	54.192	0	54.192
291+120	53.331	-85	53.246	291+620	54.137	0	54.137
291+130	53.450	-79	53.371	291+630	54.105	0	54.105
291+140	53.479	-28	53.451	291+640	54.116	0	54.116
291+150	53.471	12	53.483	291+650	54.160	0	54.160

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Station	Existing Elevation	Markup/Down mm	Proposed Elevation	Station	Existing Elevation	Markup/Down mm	Proposed Elevation
291+660	54.217	0	54.217	292+160	57.939	0	57.939
291+670	54.277	0	54.277	292+170	57.966	0	57.966
291+680	54.332	0	54.332	292+180	57.994	0	57.994
291+690	54.369	0	54.369	292+190	58.026	0	58.026
291+700	54.410	0	54.410	292+200	58.045	0	58.045
291+710	54.445	0	54.445	292+210	58.058	0	58.058
291+720	54.464	0	54.464	292+220	58.087	0	58.087
291+730	54.450	0	54.450	292+230	58.127	0	58.127
291+740	54.422	0	54.422	292+240	58.163	0	58.163
291+750	54.360	0	54.360	292+250	58.198	0	58.198
291+760	54.336	0	54.336	292+260	58.210	0	58.210
291+770	54.311	0	54.311	292+270	58.220	0	58.220
291+780	54.297	0	54.297	292+280	58.223	0	58.223
291+790	54.285	0	54.285	292+290	58.221	0	58.221
291+800	54.237	0	54.237	292+300	58.197	0	58.197
291+810	54.185	3	54.188	292+310	58.164	0	58.164
291+820	54.126	68	54.194	292+320	58.123	0	58.123
291+830	54.089	145	54.234	292+330	58.086	0	58.086
291+840	54.077	230	54.307	292+340	58.055	0	58.055
291+850	54.098	316	54.414	292+350	58.016	0	58.016
291+860	54.233	321	54.554	292+360	57.975	0	57.975
291+870	54.436	292	54.728	292+370	57.924	0	57.924
291+880	54.690	246	54.936	292+380	57.873	0	57.873
291+890	55.021	156	55.177	292+390	57.853	0	57.853
291+900	55.399	52	55.451	292+400	57.833	0	57.833
291+910	55.759	0	55.759	292+410	57.812	0	57.812
291+920	56.089	10	56.099	292+420	57.780	0	57.780
291+930	56.357	63	56.420	292+430	57.723	0	57.723
291+940	56.620	83	56.703	292+440	57.646	0	57.646
291+950	56.842	106	56.948	292+450	57.536	0	57.536
291+960	57.054	101	57.155	292+460	57.399	30	57.429
291+970	57.220	103	57.323	292+470	57.226	97	57.323
291+980	57.371	82	57.453	292+480	57.089	161	57.250
291+990	57.483	62	57.545	292+490	56.993	229	57.222
292+000	57.589	10	57.599	292+500	56.977	262	57.239
292+010	57.639	0	57.639	292+510	57.027	274	57.301
292+020	57.680	0	57.680	292+520	57.157	252	57.409
292+030	57.690	0	57.690	292+530	57.312	249	57.561
292+040	57.698	0	57.698	292+540	57.592	155	57.747
292+050	57.696	0	57.696	292+550	57.785	148	57.933
292+060	57.689	0	57.689	292+560	57.937	183	58.120
292+070	57.705	0	57.705	292+570	58.098	209	58.307
292+080	57.716	0	57.716	292+580	58.302	191	58.493
292+090	57.754	0	57.754	292+590	58.509	171	58.680
292+100	57.786	0	57.786	292+600	58.720	147	58.867
292+110	57.792	0	57.792	292+610	58.909	144	59.053
292+120	57.805	0	57.805	292+620	59.084	150	59.234
292+130	57.842	0	57.842	292+630	59.240	136	59.376
292+140	57.880	0	57.880	292+640	59.376	96	59.472
292+150	57.911	0	57.911	292+650	59.462	61	59.523

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Station	Existing Elevation	Markup/Down mm	Proposed Elevation	Station	Existing Elevation	Markup/Down mm	Proposed Elevation
292+660	59.468	61	59.529	293+160	55.721	45	55.766
292+670	59.408	56	59.464	293+170	55.791	24	55.815
292+680	59.310	44	59.354	293+180	55.865	32	55.897
292+690	59.163	42	59.205	293+190	55.970	40	56.010
292+700	58.990	60	59.050	293+200	56.071	84	56.155
292+710	58.828	67	58.895	293+210	56.172	160	56.332
292+720	58.678	62	58.740	293+220	56.304	237	56.541
292+730	58.588	28	58.616	293+230	56.487	319	56.806
292+740	58.510	24	58.534	293+240	56.681	422	57.103
292+750	58.527	-2	58.525	293+250	56.976	432	57.408
292+760	58.550	15	58.565	293+260	57.326	412	57.738
292+770	58.627	25	58.652	293+270	57.738	330	58.068
292+780	58.717	65	58.782	293+280	58.159	240	58.399
292+790	58.876	41	58.917	293+290	58.580	149	58.729
292+800	59.031	22	59.053	293+300	58.973	86	59.059
292+810	59.205	-42	59.163	293+310	59.354	28	59.382
292+820	59.371	-97	59.274	293+320	59.623	32	59.655
292+830	59.531	-147	59.384	293+330	59.860	-16	59.844
292+840	59.671	-176	59.495	293+340	59.969	7	59.976
292+850	59.801	-196	59.605	293+350	60.054	-4	60.050
292+860	59.929	-213	59.716	293+360	60.084	-10	60.074
292+870	60.071	-245	59.826	293+370	60.085	5	60.090
292+880	60.234	-297	59.937	293+380	60.058	23	60.081
292+890	60.416	-370	60.046	293+390	60.042	30	60.072
292+900	60.583	-457	60.126	293+400	60.038	26	60.064
292+910	60.762	-596	60.166	293+410	60.044	11	60.055
292+920	60.921	-756	60.165	293+420	60.069	2	60.071
292+930	61.036	-913	60.123	293+430	60.099	-12	60.087
292+940	61.071	-1030	60.041	293+440	60.116	-13	60.103
292+950	61.001	-1082	59.919	293+450	60.124	-4	60.120
292+960	60.780	-1024	59.756	293+460	60.144	-8	60.136
292+970	60.333	-781	59.552	293+470	60.147	5	60.152
292+980	59.799	-491	59.308	293+480	60.154	14	60.168
292+990	59.255	-231	59.024	293+490	60.157	40	60.197
293+000	58.577	133	58.710	293+500	60.184	85	60.269
293+010	57.938	457	58.395	293+510	60.218	192	60.410
293+020	57.383	697	58.080	293+520	60.343	252	60.595
293+030	56.949	816	57.765	293+530	60.490	335	60.825
293+040	56.584	866	57.450	293+540	60.788	311	61.099
293+050	56.395	749	57.144	293+550	61.134	283	61.417
293+060	56.249	620	56.869	293+560	61.558	188	61.746
293+070	56.221	404	56.625	293+570	61.992	73	62.065
293+080	56.191	223	56.414	293+580	62.446	-125	62.321
293+090	56.148	86	56.234	293+590	62.862	-324	62.538
293+100	56.099	-13	56.086	293+600	63.142	-425	62.717
293+110	56.005	-35	55.970	293+610	63.344	-488	62.856
293+120	55.924	-39	55.885	293+620	63.434	-478	62.956
293+130	55.846	-13	55.833	293+630	63.433	-416	63.017
293+140	55.760	2	55.762	293+640	63.379	-340	63.039
293+150	55.727	21	55.748	293+650	63.264	-241	63.023

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Station	Existing Elevation	Markup/Down mm	Proposed Elevation	Station	Existing Elevation	Markup/Down mm	Proposed Elevation
293+660	63.089	-122	62.967	294+160	59.917	37	59.954
293+670	62.882	-10	62.872	294+170	59.928	6	59.934
293+680	62.657	83	62.740	294+180	59.923	-8	59.915
293+690	62.459	138	62.597	294+190	59.882	13	59.895
293+700	62.316	137	62.453	294+200	59.822	29	59.851
293+710	62.152	162	62.314	294+210	59.756	50	59.806
293+720	62.003	201	62.204	294+220	59.738	49	59.787
293+730	61.936	191	62.127	294+230	59.733	34	59.767
293+740	61.876	207	62.083	294+240	59.743	8	59.751
293+750	61.833	240	62.073	294+250	59.757	12	59.769
293+760	61.797	298	62.095	294+260	59.785	9	59.794
293+770	61.833	318	62.151	294+270	59.826	-24	59.802
293+780	61.901	339	62.240	294+280	59.873	-55	59.818
293+790	61.973	389	62.362	294+290	59.865	-23	59.842
293+800	62.090	427	62.517	294+300	59.854	19	59.873
293+810	62.283	422	62.705	294+310	59.847	67	59.914
293+820	62.574	353	62.927	294+320	59.893	67	59.960
293+830	62.901	280	63.181	294+330	59.972	44	60.016
293+840	63.353	112	63.465	294+340	60.052	27	60.079
293+850	63.819	-66	63.753	294+350	60.130	20	60.150
293+860	64.249	-209	64.040	294+360	60.211	14	60.225
293+870	64.655	-361	64.294	294+370	60.294	6	60.300
293+880	64.943	-439	64.504	294+380	60.381	19	60.400
293+890	65.193	-523	64.670	294+390	60.443	57	60.500
293+900	65.401	-609	64.792	294+400	60.489	111	60.600
293+910	65.563	-694	64.869	294+410	60.569	131	60.700
293+920	65.662	-761	64.901	294+420	60.650	125	60.775
293+930	65.676	-786	64.890	294+430	60.717	133	60.850
293+940	65.648	-814	64.834	294+440	60.789	136	60.925
293+950	65.521	-788	64.733	294+450	60.865	135	61.000
293+960	65.332	-744	64.588	294+460	60.944	131	61.075
293+970	65.003	-604	64.399	294+470	61.023	127	61.150
293+980	64.663	-497	64.166	294+480	61.121	104	61.225
293+990	64.257	-369	63.888	294+490	61.223	82	61.305
294+000	63.829	-264	63.565	294+500	61.335	82	61.417
294+010	63.378	-179	63.199	294+510	61.445	121	61.566
294+020	62.911	-110	62.801	294+520	61.549	203	61.752
294+030	62.442	-42	62.400	294+530	61.712	263	61.975
294+040	61.982	3	61.985	294+540	61.942	293	62.235
294+050	61.523	82	61.605	294+550	62.264	268	62.532
294+060	61.129	133	61.262	294+560	62.662	205	62.867
294+070	60.767	188	60.955	294+570	63.122	116	63.238
294+080	60.534	150	60.684	294+580	63.614	28	63.642
294+090	60.350	99	60.449	294+590	64.075	-30	64.045
294+100	60.241	60	60.301	294+600	64.538	-122	64.416
294+110	60.158	30	60.188	294+610	65.017	-268	64.749
294+120	60.106	-19	60.087	294+620	65.499	-454	65.045
294+130	60.053	-31	60.022	294+630	65.979	-676	65.303
294+140	60.007	-14	59.993	294+640	66.277	-753	65.524
294+150	59.960	13	59.973	294+650	66.544	-838	65.706

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Station	Existing Elevation	Markup/Down mm	Proposed Elevation	Station	Existing Elevation	Markup/Down mm	Proposed Elevation
294+660	66.586	-734	65.852	295+160	64.294	0	64.294
294+670	66.580	-620	65.960	295+170	64.245	0	64.245
294+680	66.458	-428	66.030	295+180	64.196	0	64.196
294+690	66.313	-250	66.063	295+190	64.092	0	64.092
294+700	66.150	-92	66.058	295+200	63.974	0	63.974
294+710	66.032	-17	66.015	295+210	63.836	0	63.836
294+720	65.941	-1	65.940	295+220	63.674	0	63.674
294+730	65.870	-9	65.861	295+230	63.507	0	63.507
294+740	65.812	-12	65.800	295+240	63.429	0	63.429
294+750	65.757	0	65.757	295+250	63.418	0	63.418
294+760	65.705	0	65.705	295+260	63.400	0	63.400
294+770	65.659	0	65.659	295+270	63.386	0	63.386
294+780	65.650	0	65.650	295+280	63.403	0	63.403
294+790	65.674	0	65.674	295+290	63.453	0	63.453
294+800	65.698	0	65.698	295+300	63.599	0	63.599
294+810	65.748	0	65.748	295+310	63.749	0	63.749
294+820	65.820	0	65.820	295+320	63.861	0	63.861
294+830	65.927	0	65.927	295+330	63.955	0	63.955
294+840	65.988	0	65.988	295+340	63.969	-16	63.953
294+850	66.037	0	66.037	295+350	63.944	-56	63.888
294+860	66.009	0	66.009	295+360	63.866	-101	63.765
294+870	65.984	0	65.984	295+370	63.765	-165	63.600
294+880	65.867	0	65.867	295+380	63.583	-150	63.433
294+890	65.733	0	65.733	295+390	63.352	-85	63.267
294+900	65.563	0	65.563	295+400	63.082	28	63.110
294+910	65.404	0	65.404	295+410	62.755	230	62.985
294+920	65.269	0	65.269	295+420	62.436	459	62.895
294+930	65.153	0	65.153	295+430	62.129	709	62.838
294+940	65.073	0	65.073	295+440	61.879	937	62.816
294+950	65.006	0	65.006	295+450	61.727	1100	62.827
294+960	64.982	0	64.982	295+460	61.791	1081	62.872
294+970	64.967	0	64.967	295+470	62.005	946	62.951
294+980	64.959	0	64.959	295+480	62.470	594	63.064
294+990	64.949	0	64.949	295+490	62.929	282	63.211
295+000	64.919	0	64.919	295+500	63.306	86	63.392
295+010	64.891	0	64.891	295+510	63.674	-68	63.606
295+020	64.823	0	64.823	295+520	64.025	-170	63.855
295+030	64.751	0	64.751	295+530	64.372	-236	64.136
295+040	64.695	0	64.695	295+540	64.696	-269	64.427
295+050	64.640	0	64.640	295+550	65.016	-299	64.717
295+060	64.577	0	64.577	295+560	65.185	-205	64.980
295+070	64.534	0	64.534	295+570	65.292	-85	65.207
295+080	64.527	0	64.527	295+580	65.376	21	65.397
295+090	64.537	0	64.537	295+590	65.438	113	65.551
295+100	64.535	0	64.535	295+600	65.528	150	65.678
295+110	64.494	0	64.494	295+610	65.648	156	65.804
295+120	64.450	0	64.450	295+620	65.754	176	65.930
295+130	64.403	0	64.403	295+630	65.855	201	66.056
295+140	64.356	0	64.356	295+640	65.952	229	66.181
295+150	64.323	0	64.323	295+650	66.028	279	66.307

ALTON
PIN 10207.00
ROUTE 16 CHIP
CENTERLINE MARKUPS

Station	Existing Elevation	Markup/Down mm	Proposed Elevation	Station	Existing Elevation	Markup/Down mm	Proposed Elevation
295+660	66.167	266	66.433	296+160	73.485	0	73.485
295+670	66.329	230	66.559	296+170	73.554	0	73.554
295+680	66.468	217	66.685	296+180	73.625	0	73.625
295+690	66.595	206	66.801	296+190	73.627	0	73.627
295+700	66.766	117	66.883	296+200	73.620	0	73.620
295+710	66.892	36	66.928	296+210	73.554	0	73.554
295+720	66.852	87	66.939	296+220	73.490	0	73.490
295+730	66.802	112	66.914	296+230	73.435	0	73.435
295+740	66.751	129	66.880	296+240	73.381	0	73.381
295+750	66.737	109	66.846	296+250	73.366	0	73.366
295+760	66.905	-94	66.811	296+260	73.352	0	73.352
295+770	67.064	-287	66.777	296+270	73.344	0	73.344
295+780	67.220	-477	66.743	296+280	73.346	0	73.346
295+790	67.305	-596	66.709	296+290	73.328	0	73.328
295+800	67.260	-586	66.674	296+300	73.310	0	73.310
295+810	67.173	-532	66.641	296+310	73.284	0	73.284
295+820	66.958	-326	66.632	296+320	73.260	0	73.260
295+830	66.799	-143	66.656	296+330	73.193	0	73.193
295+840	66.490	224	66.714	296+340	73.130	0	73.130
295+850	66.191	613	66.804	296+350	73.087	0	73.087
295+860	65.902	1027	66.929	296+360	73.058	0	73.058
295+870	65.909	1178	67.087	296+370	73.066	0	73.066
295+880	66.349	929	67.278	296+380	73.068	0	73.068
295+890	66.755	747	67.502	296+390	73.091	0	73.091
295+900	67.223	537	67.760	296+400	73.129	0	73.129
295+910	67.689	363	68.052	296+410	73.117	0	73.117
295+920	68.055	321	68.376	296+420	73.123	0	73.123
295+930	68.294	441	68.735	296+430	73.232	0	73.232
295+940	68.633	493	69.126	296+440	73.302	0	73.302
295+950	69.049	502	69.551	296+450	73.498	0	73.498
295+960	69.534	466	70.000	296+460	73.721	0	73.721
295+970	70.069	381	70.450	296+470	73.990	0	73.990
295+980	70.648	247	70.895	296+480	74.266	-28	74.238
295+990	71.263	39	71.302	296+490	74.474	-58	74.416
296+000	71.787	-119	71.668	296+500	74.661	-81	74.580
296+010	72.203	-212	71.991	296+510	74.818	-105	74.713
296+020	72.403	-132	72.271	296+520	74.897	-80	74.817
296+030	72.620	-111	72.509	296+530	74.940	-49	74.891
296+040	72.770	-65	72.705	296+540	74.820	115	74.935
296+050	72.926	-67	72.859	296+550	74.681	269	74.950
296+060	73.036	-66	72.970	296+560	74.551	383	74.934
296+070	73.071	-27	73.044	296+570	74.421	468	74.889
296+080	72.980	132	73.112	296+580	74.348	466	74.814
296+090	72.996	185	73.181	296+590	74.277	432	74.709
296+100	73.043	207	73.250	296+600	74.237	337	74.574
296+110	73.194	125	73.319	296+610	74.231	178	74.409
296+120	73.345	0	73.345	296+620	74.191	24	74.215
296+130	73.365	0	73.365	296+630	74.107	-117	73.990
296+140	73.379	0	73.379	296+640	73.899	-163	73.736
296+150	73.434	0	73.434	296+650	73.596	-144	73.452

ALTON
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ROUTE 16 CHIP
CENTERLINE MARKUPS

Station	Existing Elevation	Markup/Down mm	Proposed Elevation	Station	Existing Elevation	Markup/Down mm	Proposed Elevation
296+660	73.241	-102	73.139	297+160	67.029	269	67.298
296+670	72.832	-37	72.795	297+170	66.939	274	67.213
296+680	72.397	24	72.421	297+180	66.871	295	67.166
296+690	71.965	53	72.018	297+190	66.907	248	67.155
296+700	71.536	68	71.604	297+200	66.967	213	67.180
296+710	71.137	48	71.185	297+210	67.049	194	67.243
296+720	70.723	41	70.764	297+220	67.133	208	67.341
296+730	70.263	82	70.345	297+230	67.212	237	67.449
296+740	69.821	112	69.933	297+240	67.293	264	67.557
296+750	69.402	160	69.562	297+250	67.356	309	67.665
296+760	69.005	233	69.238	297+260	67.434	339	67.773
296+770	68.659	302	68.961	297+270	67.579	302	67.881
296+780	68.335	396	68.731	297+280	67.727	262	67.989
296+790	68.109	439	68.548	297+290	67.883	212	68.095
296+800	67.901	511	68.412	297+300	68.013	171	68.184
296+810	67.748	575	68.323	297+310	68.094	161	68.255
296+820	67.673	608	68.281	297+320	68.156	151	68.307
296+830	67.719	568	68.287	297+330	68.191	149	68.340
296+840	67.860	479	68.339	297+340	68.214	140	68.354
296+850	68.129	309	68.438	297+350	68.214	135	68.349
296+860	68.427	158	68.585	297+360	68.172	153	68.325
296+870	68.757	21	68.778	297+370	68.093	190	68.283
296+880	69.090	-72	69.018	297+380	68.025	197	68.222
296+890	69.419	-119	69.300	297+390	67.972	170	68.142
296+900	69.693	-106	69.587	297+400	67.917	127	68.044
296+910	69.932	-68	69.864	297+410	67.867	59	67.926
296+920	70.139	-36	70.103	297+420	67.803	-13	67.790
296+930	70.320	-18	70.302	297+430	67.681	-43	67.638
296+940	70.455	8	70.463	297+440	67.509	-26	67.483
296+950	70.554	30	70.584	297+450	67.273	55	67.328
296+960	70.604	63	70.667	297+460	67.062	111	67.173
296+970	70.618	92	70.710	297+470	66.881	137	67.018
296+980	70.599	115	70.714	297+480	66.693	170	66.863
296+990	70.563	116	70.679	297+490	66.499	209	66.708
297+000	70.448	157	70.605	297+500	66.272	281	66.553
297+010	70.296	196	70.492	297+510	66.001	397	66.398
297+020	70.148	192	70.340	297+520	65.735	509	66.244
297+030	70.002	147	70.149	297+530	65.479	638	66.117
297+040	69.800	120	69.920	297+540	65.306	722	66.028
297+050	69.563	117	69.680	297+550	65.233	743	65.976
297+060	69.337	103	69.440	297+560	65.277	685	65.962
297+070	69.114	86	69.200	297+570	65.464	522	65.986
297+080	68.890	70	68.960	297+580	65.694	354	66.048
297+090	68.669	51	68.720	297+590	65.975	172	66.147
297+100	68.433	51	68.484	297+600	66.258	15	66.273
297+110	68.138	106	68.244	297+610	66.542	-153	66.389
297+120	67.843	161	68.004	297+620	66.825	-356	66.469
297+130	67.598	172	67.770	297+630	67.070	-560	66.510
297+140	67.373	203	67.576	297+640	67.322	-809	66.513
297+150	67.185	234	67.419	297+650	67.592	-1114	66.478

ALTON
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ROUTE 16 CHIP
CENTERLINE MARKUPS

Station	Existing Elevation	Markup/Down mm	Proposed Elevation	Station	Existing Elevation	Markup/Down mm	Proposed Elevation
297+660	67.840	-1435	66.405				
297+670	68.054	-1760	66.294				
297+680	68.198	-2054	66.144				
297+690	68.254	-2297	65.957				
297+700	68.182	-2450	65.732				
297+710	67.950	-2483	65.467				
297+720	67.545	-2380	65.165				
297+730	66.917	-2090	64.827				
297+740	66.176	-1689	64.487				
297+750	65.286	-1107	64.179				
297+760	64.465	-561	63.904				
297+770	63.734	-69	63.665				
297+780	63.161	301	63.462				
297+790	62.795	498	63.293				
297+800	62.640	519	63.159				
297+810	62.746	314	63.060				
297+820	62.895	99	62.994				
297+830	63.112	-150	62.962				
297+840	63.226	-261	62.965				
297+850	63.205	-230	62.975				
297+860	63.133	-178	62.955				
297+870	62.982	-79	62.903				
297+880	62.835	-12	62.823				
297+890	62.702	4	62.706				
297+895	62.625	0	62.625				

CONSTRUCTION NOTES

Item 201.23 Removing Single Tree Top Only

Single tree tops are to be removed at the following locations:

Station	Side
291+740	Lt
294+372	Lt
294+385	Lt.
294+442	Lt
294+823	Lt
297+623	Lt

Stumps will be removed by appropriate contract items, as directed by the Resident.

Item 202.203 Pavement Butt Joints

Grind butt joints on all paved drives, entrances and side roads as directed by the Resident.

Item 203.2001 Common Excavation (Plan Quantity)

Station		Station	Description
290+660	To	290+720	Profile Cut & Ditch
290+960	To	291+180	Profile Cut & Ditch
291+780	To	291+940	Profile Cut & Ditch
292+480	To	292+620	Profile Cut & Ditch
292+800	To	293+080	Profile Cut & Ditch
293+560	To	294+040	Profile Cut & Ditch
294+400	To	294+760	Profile Cut & Ditch
295+400	To	295+500	Profile Cut & Ditch
295+620	To	295+980	Profile Cut & Ditch
296+720	To	297+895	Profile Cut & Ditch

Also includes all Driveways

Item 211.21 Inslope Rehabilitation

Left			Right		
290+720	To	290+824	290+720	To	290+734
290+832	To	290+943	290+742	To	290+790
290+953	To	290+960	290+790	To	290+960
291+186	To	291+215	291+180	To	291+240
291+273	To	291+780	291+290	To	291+780
291+940	To	291+991	292+010	To	292+144
291+997	To	292+363	292+370	To	292+421

CONSTRUCTION NOTES

Alton
PIN 10207.00
Route 16 CHIP

292+771	To	292+800	292+680	To	292+780
293+080	To	293+315	293+120	To	293+280
293+387	To	293+560	293+360	To	293+510
294+306	To	294+344	294+040	To	294+059
295+205	To	295+327	294+077	To	294+088
295+725	To	295+760	294+110	To	294+313
296+405	To	296+445	294+318	To	294+400
			294+760	To	294+853
			294+865	To	295+375
			295+980	To	296+066
			296+080	To	296+107
			296+354	To	296+380
			296+528	To	296+562
			296+575	To	296+622

Precise locations will be determined in the field by the Resident.

Item 211.30 Ditch Excavation

Left			Right		
292+369	To	292+480	291+240	To	291+290
294+050	To	294+211	292+780	To	292+800
294+220	To	294+297	293+080	To	293+108
294+344	To	294+362	295+500	To	295+620
294+370	To	294+400	296+107	To	296+194
294+760	To	294+871	296+203	To	296+230
294+889	To	295+205	296+240	To	296+296
295+130	To	Ditch outlet	296+305	To	296+354
295+230	To	Ditch outlet	296+380	To	296+392
295+327	To	295+400	296+406	To	296+467
295+500	To	295+620	296+474	To	296+500
295+980	To	296+080	296+667	To	296+708
296+122	To	296+405	296+717	To	296+720
296+516	To	296+739			

Precise locations will be determined in the field by the Resident.

CONSTRUCTION NOTES

Alton

PIN 10207.00

Route 16 CHIP

Item 211.40 New Ditch Excavation

Left			Comments	Right			Comments
291+230	To	291+273		291+940	To		New Outlet
292+620	To	292+771		291+940	To	292+010	
292+771	To		New Outlet	292+144	To		New Outlet
293+315	To		New Outlet	292+144	To	292+370	
293+315	To	293+387		292+370	To		New Outlet
293+387	To		New Outlet	292+421	To	292+480	
296+445	To	296+479		292+480	To		New Outlet
296+482	To	296+510		292+620	To		New Outlet
				292+620	To	292+680	
				292+680	To		New Outlet
				293+280	To		New Outlet
				293+280	To	293+360	
				293+360	To		New Outlet
				293+510	To		New Outlet
				293+510	To	293+560	
				295+375	To	295+400	
				296+500	To	296+520	

Precise locations will be determined in the field by the Resident.

Item 304.104 Aggregate Subbase Course - Gravel, Plan Quantity

Station		Station	Description
290+660	To	290+720	From Cross-sections
290+720	To	290+960	Variable Gravel
290+960	To	291+180	From Cross-sections
291+180	To	291+780	Variable Gravel
291+780	To	291+940	From Cross-sections
291+940	To	292+480	Variable Gravel
292+480	To	292+620	From Cross-sections
292+620	To	292+800	Variable Gravel
292+800	To	293+080	From Cross-sections
293+080	To	293+560	Variable Gravel
293+560	To	294+040	From Cross-sections
294+040	To	294+400	Variable Gravel
294+400	To	294+760	From Cross-sections
294+760	To	295+400	Variable Gravel

CONSTRUCTION NOTES

Alton
PIN 10207.00
Route 16 CHIP

295+400 To 295+500 From Cross-sections
295+500 To 295+620 Variable Gravel
295+620 To 295+980 From Cross-sections
295+980 To 296+720 Variable Gravel
296+720 To 297+895 From Cross-sections
Also Argyle Road and all Driveways

Item 310.23 75 mm Plant Mix Recycled Asphalt Pavement

A 75 mm (compacted depth) layer of PMRAP will be placed in the following locations. PMRAP will be placed 300 mm beyond face of curb and to the face of guardrail in areas with curb and guardrail as depicted on the typical. A bypass lane shall be placed tapering from 0 m at station 296+740 to 3.3 m at station 296+780 maintaining 3.3 m until it tapers to 0 m from 296+840 to 296+900.

	Station	Width	Length
	290+660	8.4 +/-	6080.00
Taper	296+740	8.4 +/-	50.00
Bypass Lane	296+790	11.4 +/-	50.00
Taper	296+840	11.4 +/-	50.00
	296+890	8.4 +/-	1000.00
	297+890	8.4 +/-	

Item 403.209 Hot Mix Asphalt 9.5 mm (Incidentals)

29 Paved Drives
43 Gravel Drives w/ Paved Lips (1.0 m)
1 Paved Island

Note: Approximate stations are available upon request to Division 3 office.

CONSTRUCTION NOTES

Alton

PIN 10207.00

Route 16 CHIP

Item 403.210 Hot Mix Asphalt 9.5 mm Surface

9.5 mm HMA shall be placed in one 30 mm lift.

Shoulder width shall be 1.2 M where curb or guardrail is placed on one side and 1.5 M where curb or guardrail is placed on both sides, unless otherwise noted.

An additional width of 0.6 M shall be paved in areas of guardrail and 0.3M for curb areas. 350 flared terminal widening shall have 9.5 mm surface.

Item 403.213 Hot Mix Asphalt 12.5 mm, Base

12.5 mm HMA shall be placed in one 45 mm lift.

Shoulder width shall be 1.2 M where curb or guardrail is placed on one side and 1.5 M where curb or guardrail is placed on both sides, unless otherwise noted.

An additional width of 0.6 M shall be paved in areas of guardrail and 0.3M for curb areas. 350 flared terminal widening shall have 12.5 mm base.

Item 409.15 Bituminous Tack Coat Applied

A bituminous tack coat shall be applied between all lifts of pavement including The Plant Mixed Recycled Asphalt Pavement.

Item 411.10 Untreated Aggregate Surface Course, Truck Measure

To back up paved lips on gravel driveways (50mm thick) and as directed by Resident.

Item 603.16 375 mm Culvert Pipe Option I

Left		Right	
Station	Length	Station	Length
1+012.5	9	291+026	11
291+090	9.0	291+097	10
292+954	19.0	292+507	10
294+215	8	293+113	12.0
294+300	9	294+010	20.0
294+449	10.0	295+726	20
295+473	10	297+095	8
296+482	9		
296+567	8		
296+825	15		

Extend existing pipe to basin

CONSTRUCTION NOTES

Alton

PIN 10207.00

Route 16 CHIP

Item 603.179 450 mm Culvert Pipe Option III

Station	Length	Notes
294+700	14.1	Cross-Pipe
297+547.5	14.7	Cross-Pipe
297+796	14.2	Cross-Pipe

Item 603.19 600 mm Culvert Pipe Option I

Station	Length	Notes
296+825	21.2	Outlet from Catch Basin

Item 603.199 600 mm Culvert Pipe Option III

Station	Length	Notes
296+825	21.2	Cross-Pipe into Catchbasin

Item 603.289 2100 mm Culvert Pipe Option III

Replacing extensions on box culvert at station 290+787, approximately 3.0 m on the left and 3.2 m on the right.

Item 603.7315 Remove and Relay 375 mm Metal Pipe

Station	Length	Side
290+828	10.0	Lt
294+876	18.2	Lt

Additional areas as determined by the Resident.

Item 604.092 Catch Basin Type B1-C

Station 296+825 Lt.

Item 604.244 Catch Basin Type F-4

Station 295+459 Rt.

This catch basin is to be used as a drop inlet into a pipe that outlets water through a berm.

CONSTRUCTION NOTES**Item 605.09 150 mm Underdrain Type B**

<u>Station</u>	<u>To</u>	<u>Station</u>	<u>Station</u>	<u>To</u>	<u>Station</u>
293+900	To	294+060Lt.	292+860	To	293+000Rt.
294+560	To	294+695Lt.	293+900	To	294+000Rt.
297+671	To	297+770Lt.	295+780	To	295+860Rt.
			297+610	To	297+796Rt.

Item 606.23 Guardrail Type 3C – Single Rail

<u>Left</u>			<u>Right</u>		
290+740	To	290+820	290+748	To	290+813
291+790	To	291+920	293+358	To	293+457
292+500	To	292+580	295+641	To	295+714
293+005	To	293+169	295+843	To	295+958
293+620	To	293+750	296+787	To	296+867
295+212	To	295+223	297+142	To	297+275
295+319	To	295+346			
295+711	To	295+822			
297+132	To	297+273			
297+465	To	297+671			
297+770	To	297+816			

Item 606.231 Guardrail Type 3C – 4.5 M Radius or Less

<u>Station</u>		<u>Station</u>	<u>Side</u>	<u>Quantity</u>
290+743	To	290+745	Rt	1
290+824	To	290+825	Lt	1
292+580	To	292+585	Lt	2

Item 606.232 Guardrail Type 3C – Over 4.5 M Radius

<u>Station</u>		<u>Station</u>	<u>Side</u>	<u>Quantity</u>
290+745	To	290+748	Rt	1
290+820	To	290+824	Lt	1
295+714	To	295+719	Rt	2
295+838	To	295+843	Rt	2
296+782	To	296+787	Rt	2
297+127	To	297+132	Lt	2
297+275	To	297+280	Rt	2
297+273	To	297+278	Lt	2
297+460	To	297+465	Lt	2
297+671	To	297+676	Lt	2
297+816	To	297+821	Lt	2

CONSTRUCTION NOTES

Item 606.265 Terminal End – Single Rail – Galvanized Steel

Station	Side	Station	Side
290+825	Lt	290+743	Rt
292+585	Lt	295+719	Rt
297+127	Lt	295+838	Rt
297+278	Lt	296+782	Rt
297+460	Lt	297+280	Rt
297+676	Lt		
297+821	Lt		

Item 606.364 Guardrail - Remove, Modify and Reset Type 3B

Station		Station	Side
295+223	To	295+319	Lt

Note: This item will be used to modify guardrail to type 3C

Item 606.47 Single Wood Post

To replace mailbox posts as directed by Resident

Item 606.754 Widen Shoulder For Guardrail 350 Flared Terminal

Left			Right		
290+707	To	290+740	290+813	To	290+846
291+757	To	292+790	293+325	To	293+358
291+920	To	291+953	293+457	To	293+490
292+467	To	292+500	295+608	To	295+641
292+972	To	293+005	295+958	To	295+991
293+169	To	293+202	297+109	To	297+142
293+587	To	293+620			
293+750	To	293+783			
295+179	To	295+212			
295+346	To	295+379			
295+678	To	295+711			
295+822	To	295+855			
296+867	To	296+900			
297+671	To	297+704			
297+737	To	297+770			

Exact locations to be determined in the field by the Resident.

CONSTRUCTION NOTES

Item 606.79 Guardrail 350 Flared Terminal

Left			Right		
290+728	To	290+740	290+813	To	290+825
291+778	To	291+790	293+346	To	293+358
291+920	To	291+932	293+457	To	293+469
292+488	To	292+500	295+629	To	295+641
292+993	To	293+005	295+958	To	295+970
293+169	To	293+181	297+130	To	297+142
293+608	To	293+620			
293+750	To	293+762			
295+200	To	295+212			
295+346	To	295+358			
295+699	To	295+711			
295+822	To	295+834			
296+867	To	296+879			
297+671	To	297+683			
297+758	To	297+770			

Exact locations to be determined in the field by the Resident.

Item 609.31 Curb Type 3

Left			Right			Notes
293+900	To	294+040	292+880	To	292+980	
294+350	To	294+362	293+900	To	293+980	
294+369	To	294+390	295+780	To	295+840	
294+580	To	294+678	296+631	To	296+658	
296+095	To	296+105	297+610	To	297+702	
296+095	To	296+105	297+713	To	297+740	
297+435	To	297+441	297+823	To	297+833	Island single run
297+671	To	297+740	297+842	To	297+872	
297+826	To	297+840	297+842	To	297+858	
297+850	To	297+858				

Item 610.08 Plain Riprap

To be used at all pipe outlets and cross pipe inlets.

CONSTRUCTION NOTES

Alton
PIN 10207.00
Route 16 CHIP

Item 613.319 Erosion Control Blanket (1.2 m wide)

It is intended to apply this item in all areas of Ditch (Item 211.30), New Ditch (Item 211.40), ditches in full construction sections and as directed by the Resident. Blanket May be used on slopes, entrance radii and other misc. locations.

Item 615.07 Loam

In areas where method 1 seeding will be used and as directed by the Resident.

Item 620.58 Erosion Control Geotextile

All areas that receive Riprap (Item 610.08) will receive erosion control geotextile.

Item 627.75 White or Yellow Pavement & Curb Marking

To be used on the ends of the island located from Station 296+095 to 296+105.

Item 627.76 Temporary Pavement Mark Line, White or Yellow

To be used once on the PMRAP and on each lift of HMA pavement. RPM's or a painted line will be used at the contractor's discretion upon approval from Resident.

Item 631 Hourly Rental Items

As directed by the Resident.

GENERAL NOTES

Alton

PIN 10207.00

Route 16 CHIP

1. All joints between existing and proposed hot bituminous pavement shall be butted. Payment shall be made under Item 202.203 Pavement Butt Joint.
2. Construct Butt Joints at all paved drives and entrances.
3. Where deemed necessary by the Resident, winter sand shall be removed from the edges of shoulders and placed in designated areas or disposed of. Payment will be made under the appropriate contract items.
4. All inslopes and ditches in cut areas shall be regraded to 1:3, or flatter, as directed by the Resident.
5. The Contractor shall place suitable existing material, or other material acceptable to the Resident, on all pavement edges to allow no greater than a 40 mm [1 ½ in] drop-off and be graded to 1:3 or flatter. Payment to be incidental to the contract.
6. Waste material up to approximately 1000 yd³ shall be disposed of in a waste area designated on an adjacent project. This project is PIN 11076.00 Lagrange/Howland - Route 155. The site is located on Route 155, ¼ of a mile East of the intersection of Routes 16 and 155. Coordination of this waste site will be done through the Resident. Waste material over and above the 1000 yd³ shall be disposed of off the project in waste areas approved by the Resident.
7. Any damage to the slopes caused by the Contractor's equipment, personnel, or operation shall be repaired to the satisfaction of the Resident. All work, equipment and materials required to make repairs shall be at the Contractor's expense.
8. A one meter [3 ft] paved lip shall be placed at all gravel entrances, except woods and field entrances, unless otherwise directed by the Resident.
9. Item # 411.10, Untreated Aggregate Surface Course, may also meet the gradation requirements of item # 204.20, Add Shoulder Aggregate.
10. Any necessary cleaning of existing pavement prior to paving shall be incidental to the related paving items.
11. No existing drainage shall be abandoned, removed or plugged without prior approval of the Resident.
12. The following shall be incidental to the 603 item(s):
 - a) Any cutting of existing culverts and or connectors necessary to install new culvert replacements or extensions
 - b) All pipe excavation including any cutting and removal of pavement

GENERAL NOTES

Alton

PIN 10207.00

Route 16 CHIP

- c) All ditching at pipe ends
 - d) Furnishing, placing, grading, and compacting of any new gravel and/or fill material including Granular Borrow used under pipes and for temporary detours to maintain traffic during pipe installation (excavation is also incidental).
 - e) Granular Borrow under the pipe shall meet the requirements for Underwater Backfill
 - f) All work necessary to connect to existing pipes
 - g) Flow lines may be changed by 0.5 m [1.5 ft]
 - h) Any necessary clearing of brush and small trees at culvert ends
13. Existing culverts and catch basins will be cleaned as directed by the Resident under the appropriate Pay Items.
14. As directed by the Resident, all existing Underdrain Outlets shall be located, cleaned out, and ditched as required or replaced as necessary. Payment will be made under appropriate hourly contract items.
15. Two guardrail delineator posts will be installed at the leading end and one at the trailing end of each run of guardrail. One delineator post will also be installed at each underdrain outlet.
16. Reflectorized silver white beam guardrail delineators shall be mounted on all new, modified, removed modified and reset, or removed and reset guardrail. Delineators shall be installed on every tenth post in a tangent and every fifth post on curves. Reflectorized beam guardrail delineators shall meet the requirements of section 719.01. The delineators shall be mounted on the guardrail beam at the posts. Beam guardrail delineators will not be paid for directly, but will be considered incidental to the guardrail items.
17. All wood posts on 350 terminal end installation shall be cut so that no more than 25 mm [1 in] is exposed above the beam.
18. 350 Flared Terminals shall be installed concurrently with the placement of each section of beam guardrail.
19. Guardrail which is removed and not reused on the project becomes property of the Contractor.
20. Holes created by Guardrail removal will be filled and compacted with approved materials as directed by the Resident. Payment to be considered incidental to the guardrail items.
21. Backing up bituminous curb is incidental to the curb items. In areas where new bituminous curb is designated to replace existing, the removal of the old bituminous curb shall be incidental to the new curb.
22. Trim all tree branches to 6 m [20 ft] above pavement. Payment shall be made under the appropriate labor and equipment rental items.

GENERAL NOTES

Alton
PIN 10207.00
Route 16 CHIP

23. White pavement/curb marking (Item 627.75) shall be applied to all island tapered ends.
24. "Undetermined Locations" shall be determined by the Resident.
25. Stations referenced are approximate.
26. All work shall be done in accordance with the Maine Department of Transportation's Best Management Practices for Erosion & Sediment Control, January, 2000.
27. MDOT will final stripe the project. The Contractor is responsible for transferring the existing striping pattern to the surface course. In addition to the centerline the state will stripe a white lane dividing line for the bypass lane from Station 296+780 to 296+840.
28. Any gravel necessary for maintenance of traffic will be considered incidental to the project.
29. Any useable guardrail posts and beams from any existing guardrail being removed shall be used to replace any unusable guardrail posts and beams in the remove and reset guardrail areas.
30. All millings left over shall become property of the State and shall be stockpiled in the MDOT maintenance lot on Route 16. The material shall be hauled and stockpiled in one area, pushed up by a loader or similar vehicle. Such stockpiling shall be considered incidental to the contract.
31. Before any removal of pavement surface or paving can begin, all project stationing must be clearly marked out every 20 m with either flats or paint. If painted on the roadway, stations must be marked on each lift of pavement.
33. Any wintered base pavement will require Temporary Pavement markings of paint, both yellow centerline and white edge lines and will be considered part of Item # 627.76 Temporary Pavement Marking Lines, White or Yellow.
34. Areas requiring fill on project will come from suitable excavation from the project Excavation Items.
35. Dust control will not be paid for directly, but will be considered incidental to the Erosion Control Plan. (SEWPCP)
36. All pavement milling areas will have all pavement removed with minimal contamination, any remaining material not broken up will require reclaiming or excavation, existing Shoulder material shall be graded (to existing gravel grade) smooth to drain. **At no time** will pavement be removed from more than 3621M (2.5 miles) of the project, until at least Plant Mix Recycle Asphalt Pavement is placed on previously milled 3621M section.

GENERAL NOTES

Alton

PIN 10207.00

Route 16 CHIP

37. All roadway drainage installation trenches will require 75mm minimum of 12.5 mm, base pavement same day or as directed by the resident. Payment will be incidental to the contract as per Standard Specification 105.4.1.
38. The contractor is responsible for the careful **side staking** of existing centerline as per standard specification 105.6.2. Side stakes shall be placed safely outside of the construction limits and the existing centerline grades shall be transferred to these stakes. These stakes and grades will be used to layout centerline and determine new construction finish grades from differential elevation sheets furnished. All layout, stakes, and grades will be checked and must be acceptable to the Resident.
39. All Gravel (304.104) will be paid **PLAN QUANTITY**. The contractor is responsible to verify the quantities and adjust his or her bid accordingly. This quantity includes all gravel required to bring mainline and shoulders to design slopes and elevations given and a 3:1 minimum in-slope, gravel to fill pavement milling areas, variable gravel sections, gravel for full construction areas listed in construction notes, maintenance of traffic gravel in full construction sections, extra width guardrail sections, match all existing drives, entrances, and side roads to the new roadway elevations, existing widths shown are minimums, with new acceptable profile to match existing entrances, as directed by the resident, and all shoulder widenings. All computation methods and quantities used for Engineers Estimate are available by contacting Division 3 Project Manager Janis Piper at 207-941-4754.
40. All clearing necessary to complete the project including but not limited to, ditching areas, backslope excavation areas, Full Construction Sections, and any designated fill areas shall be considered incidental to the contract. All clearing shall be 1.5 M beyond construction limits. The contractor will be responsible for notifying property owners prior to cutting trees, and property owners will have first refusal on any wood.
41. A copy of the soils report for this project is available by contacting the project manager, Janis Piper, at 207-941-4754.

General Decision Number ME030010 06/13/2003 ME10

Superseded General Decision No. ME020010

State: Maine

Construction Type:
HIGHWAY

County(ies):
PENOBSCOT

HIGHWAY CONSTRUCTION PROJECTS excluding major bridging
(for example: bascule, suspension and spandrel arch bridges;
those bridging waters presently navigating or to be navigable;
and those involving marine construction in any degree); tunnels,
building structures in rest area projects and railroad
construction.

Modification Number Publication Date
0 06/13/2003

COUNTY(ies):
PENOBSCOT

ENGI0004M 04/01/2003

	Rates	Fringes
POWER EQUIPMENT OPERATORS:		
Grader	16.51	6.00
Paver	16.51	6.00
Roller	16.51	6.00

SUME4026A 10/24/2000

	Rates	Fringes
CARPENTER		
Including Form Work	11.19	1.72
CEMENT MASON/FINISHERS	9.13	
IRONWORKERS		
Stuctural	17.50	1.70
LABORERS		
Landscape	7.84	
Rakers	10.18	2.14
Unskilled	8.73	1.71
POWER EQUIPMENT OPERATORS		
Backhoes	11.81	1.88
Bulldozers	13.12	2.72
Cranes	15.25	1.70
Excavators	11.69	2.40
Loaders	12.21	3.19
TRUCK DRIVERS		
Dump	9.27	
Two Axle	9.12	1.63
Tri Axle	10.63	2.11

WELDERS - Receive rate prescribed for craft performing operation to which welding is incidental.

=====

Unlisted classifications needed for work not included within the scope of the classifications listed may be added after award only as provided in the labor standards contract clauses (29 CFR 5.5(a)(1)(ii)).

In the listing above, the "SU" designation means that rates listed under that identifier do not reflect collectively bargained wage and fringe benefit rates. Other designations

indicate unions whose rates have been determined to be prevailing.

WAGE DETERMINATION APPEALS PROCESS

1.) Has there been an initial decision in the matter? This can be:

- * an existing published wage determination
- * a survey underlying a wage determination
- * a Wage and Hour Division letter setting forth a position on a wage determination matter
- * a conformance (additional classification and rate) ruling

On survey related matters, initial contact, including requests for summaries of surveys, should be with the Wage and Hour Regional Office for the area in which the survey was conducted because those Regional Offices have responsibility for the Davis-Bacon survey program. If the response from this initial contact is not satisfactory, then the process described in 2.) and 3.) should be followed.

With regard to any other matter not yet ripe for the formal process described here, initial contact should be with the Branch of Construction Wage Determinations. Write to:

Branch of Construction Wage Determinations
Wage and Hour Division
U. S. Department of Labor
200 Constitution Avenue, N. W.
Washington, D. C. 20210

2.) If the answer to the question in 1.) is yes, then an interested party (those affected by the action) can request review and reconsideration from the Wage and Hour Administrator (See 29 CFR Part 1.8 and 29 CFR Part 7). Write to:

Wage and Hour Administrator
U.S. Department of Labor
200 Constitution Avenue, N. W.
Washington, D. C. 20210

The request should be accompanied by a full statement of the interested party's position and by any information (wage payment data, project description, area practice material, etc.) that the

requestor considers relevant to the issue.

3.) If the decision of the Administrator is not favorable, an interested party may appeal directly to the Administrative Review Board (formerly the Wage Appeals Board). Write to:

Administrative Review Board
U. S. Department of Labor
200 Constitution Avenue, N. W.
Washington, D. C. 20210

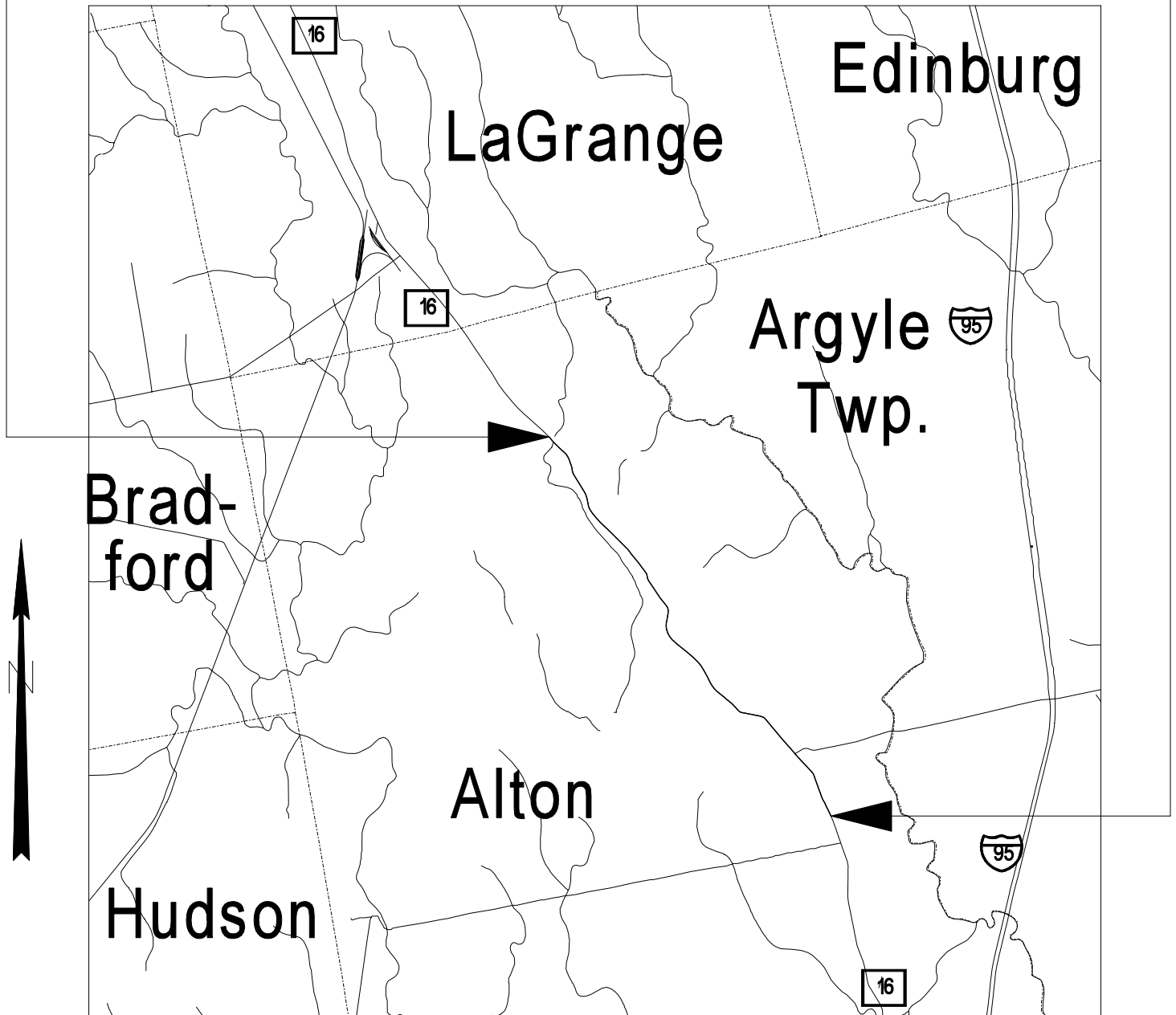
4.) All decisions by the Administrative Review Board are final.
END OF GENERAL DECISION

□

PROJECT NO. STP-1020(700)X

BEGIN PROJECT STA. 290+660

END PROJECT STA. 297+895



LOCATION MAP



Scale in Kilometers

SPECIAL PROVISION
CONSTRUCTION AREA

A Construction Area located in the **Town of Alton** has been established by the Maine Department of Transportation in accordance with provisions of Title 29, Section 1703, Maine Revised Statutes Annotated.

The section of highway under construction in Penobscot County, project STP-1020(700)X is located on Route 16, beginning 1.37 km (0.85 mi) southerly of the LaGrange town line and extending southerly 7.235 km (4.50 mi).

The State Department of Transportation or the State's Engineer may issue permits for stated periods of time for moving construction equipment without loads, low-bed trailers with overloads, over-height, over-width or over-length equipment or materials over all State maintained sections described in the "Construction Area" above and in addition may issue permits for stated periods of time for moving overweight vehicles and loads over the section described in (a) above. The right to revoke such a permit at any time is reserved by the State Department of Transportation and the issuance of such permits shall be subject to any Special Provisions or Supplemental Specifications written for this project.

A Temporary Permit for each move may be issued by the State Department of Transportation or the State's Engineer for moving Contractor's construction equipment used on the project which exceeds the legal limits (shovels, bulldozers, etc.) to sources of construction material over highways maintained by the State reasonably within the area of the project.

The Municipal Officers for the **Town of Alton** agreed that a permit will be issued to the Contractor for the purpose of hauling loads in excess of the limits as specified in Title 29, Maine Revised Statutes Annotated, on the town ways as described in the "Construction Area" and that single move permits will be issued for moving Contractor's construction equipment used on the project which exceeds the legal limits (shovels, bulldozers, etc.) to sources of construction material over town ways reasonably within the area of the project.

In the event it is necessary to transport gravel, borrow, or other construction material in legally registered vehicles carrying legal loads over town ways, a Contractor's Bond of not more than Nine Thousand (\$9,000.00) per kilometer of traveled length may be required by the town, the exact amount of said bond to be determined prior to use of any town way.

The maximum speed limits for trucks on any town way will be forty (40) km per hour [25 mph], unless a higher legal limit is specifically agreed upon in writing by the Municipal Officers concerned.

SPECIAL PROVISION
CONSTRUCTION AREA

Title 29A, M.R.S.A., Subsection 2383. Overlimit movement permits

1. Overlimit movement permits issued by State. The Secretary of State, acting under guidelines and advice of the Commissioner of Transportation, may grant permits to move non-divisible objects having a length, width, height or weight greater than specified in this Title over a way or bridge maintained by the Department of Transportation.
2. Permit Fee. The Secretary of State, with the advice of the Commissioner of Transportation, may set the fee for these permits, at not less than \$3, nor more than \$15, based on weight, height, length and width.
3. County and municipal permits. A permit may be granted, for a reasonable fee, by county commissioners or municipal officers for travel over a way or bridge maintained by that county or municipality.
4. Permits for weight. A vehicle granted a permit for excess weight must first be registered for the maximum gross vehicle weight allowed for that vehicle.
5. Special mobile equipment. The Secretary of State may grant a permit, for no more than one year, to move pneumatic-tire equipment under its own power, including Class A and Class B special mobile equipment, over ways and bridges maintained by the Department of Transportation. The fee for that permit is \$15 for each 30-day period.
6. Scope of permit. A permit is limited to the particular vehicle or object to be moved and particular ways and bridges.
7. Construction permits. A permit for a stated period of time may be issued for loads and equipment employed on public way construction projects, United States Government projects or construction of private ways, when within construction areas established by the Department of Transportation. The Permit:
 - A. Must be procured from the municipal officers for a construction area within that municipality;
 - B. May require the Contractor to be responsible for damage to ways used in the construction areas and may provide for:
 - (1) Withholding by the agency of the work of final payment under contract;
or
 - (2) The furnishing of a bond by the Contractor to guarantee suitable repair or payment of damages.
 - C. May be granted by the Department of Transportation or by the state engineer in charge of the construction contract; and
 - D. For construction areas, carries no fee and does not come within the scope of this section.
8. Gross vehicle weight permits. The following may grant permits to operate a vehicle having a gross vehicle weight exceeding the prescribed limit:

- A. The Secretary of State, with the consent of the Department of Transportation, for state and state aid highways and bridges within city or compact village limits;
 - B. Municipal officers, for all other ways and bridges within that city and compact village limits; and
 - C. The county commissioners, for county roads and bridges located in unorganized territory.
9. Pilot vehicles and state police escorts. Pilot vehicles required by a permit must be equipped with warning lights and signs as required by the Secretary of State with the advice of the Department of Transportation.

Warning lights may only be operated and lettering on the signs may only be visible on a pilot vehicle while it is escorting on a public way a vehicle with a permit.

The Secretary of State shall require a State Police escort for a single vehicle or a combination of vehicles of 125 feet or more in length or 16 feet or more in width. The Secretary of State, with the advice of the Commissioner of Transportation, may require vehicles of lesser dimensions to be escorted by the State Police.

The Bureau of State Police shall establish a fee for State Police escorts.

All fees collected must be used to defray the cost of services provided.

With the advice of the Commissioner of Transportation and the Chief of the State Police, the Secretary of State shall establish rules for the operation for the operation of pilot vehicles.

10. Taxes paid. A permit for a mobile home may not be granted unless the applicant provides reasonable assurance that all property taxes, sewage disposal charges and drain and sewer assessments applicable to the mobile home, including those for the current tax year, have been paid or that the mobile home is exempt from those taxes.

1993, c. 683, § S-2, eff. January 1, 1995.

Historical and Statutory Notes

Derivation:

R.S. 1954, c. 22 § 98
Laws 1955, c. 389
Laws 1967, c. 3.
Laws 1971, c. 593, § 22.
Laws 1973, c. 213.
Laws 1975, c. 130, §
Laws 1975, c. 319, § 2

Laws 1977, c. 73, § 5.
Laws 1981, c. 413.
Laws 1985, c. 225, § 1
Laws 1987, c. 52.
Laws 1987, 781, § 3.
Laws 1989, c. 866, § B-13.
Laws 1991, c. 388, § 8.
Laws 1993, c. 683, § A-1.
Former 29 M.R.S.A. § 2382.

Cross Reference

Collection by Secretary of State, See 29-A
M.R.S.A. § 154.

SPECIAL PROVISION
(Consolidated Special Provisions)

SPECIAL PROVISION SECTION 101
CONTRACT INTERPRETATION

101.2 Definitions - Closeout Documentation

Replace the sentence “A letter stating the amount..... DBE goals.” with “DBE Goal Attainment Verification Form”

SPECIAL PROVISION SECTION 102
DELIVERY OF BIDS
(Location and Time)

102.7.1 Location and Time Add the following sentence “As a minimum, the Bidder will submit a Bid Package consisting of the Notice to Contractors, the completed Acknowledgement of Bid Amendments & Submission of Bid Bond Validation Number form, the completed Schedule of Items, 2 copies of the completed Agreement, Offer, & Award form, a Bid Bond or Bid Guarantee, and any other Certifications or Bid Requirements listed in the Bid Book.”

SPECIAL PROVISION SECTION 103
AWARD AND CONTRACTING

103.3.1 Notice and Information Gathering

Change the first paragraph to read as follows: “After Bid Opening and as a condition for Award of a Contract, the Department may require an Apparent Successful Bidder to demonstrate to the Department’s satisfaction that the Bidder is responsible and qualified to perform the Work.”

SPECIAL PROVISION SECTION 105
GENERAL SCOPE OF WORK

105.6.2 Contractor Provided Services

Change the first paragraph by the addition of the following as the second sentence: “The Contractor is also responsible for providing construction centerline, or close reference points, for all Utility Facilities relocations and adjustments as necessary to complete the Work.”

SPECIAL PROVISION SECTION 106 QUALITY

106.6 Acceptance Add the following to paragraph 1 of A: “This includes Sections 401 - Hot Mix Asphalt, 402 - Pavement Smoothness, and 502 - Structural Concrete - Method A - Air Content.”

Add the following to the beginning of paragraph 3 of A: “For pay factors based on Quality Level Analysis, and”

SPECIAL PROVISION SECTION 107 TIME

107.3.1 General Add the following: “If a Holiday occurs on a Sunday, the following Monday shall be considered a Holiday. Sunday or Holiday work must be approved by the Department, except that the Contractor may work on Martin Luther King Day, President’s Day, Patriot’s Day, the Friday after Thanksgiving, and Columbus Day without the Department’s approval.”

SPECIAL PROVISION SECTION 108 PAYMENT

108.4 Payment for Materials Obtained and Stored First paragraph, second sentence, delete the words “...Delivered on or near the Work site at acceptable storage places.”

SPECIAL PROVISION SECTION 109 CHANGES

109.1.1 Changes Permitted Add the following to the end of the paragraph: “There will be no adjustment to Contract Time due to an increase or decrease in quantities, compared to those estimated, except as addressed through Contract Modification(s).”

109.1.2 Substantial Changes to Major Items Add the following to the end of the paragraph: “Contract Time adjustments may be made for substantial changes to Major Items when the change affects the Critical Path, as determined by the Department”

109.4.4 Investigation / Adjustment In the third sentence, delete the words “subsections (A) - (E)”

109.7.2 Basis of Payment Replace with the following: “Equitable Adjustments will be established by mutual Agreement for compensable items listed in Section 109.7.3-Compensable Items, based upon Unit or Lump Sum Prices. If Agreement cannot be reached, the Contractor shall accept payment on a Force Account basis as provided in Section 109.7.5 - Force Account Work, as full and complete compensation for all Work relating to the Equitable Adjustment.”

109.7.3 Compensable Items Replace with the following: “The Contractor is entitled to compensation for the following items, with respect to agreed upon Unit or Lump Sum Prices:

1. Labor expenses for non-salaried Workers and salaried foremen.
2. Costs for Materials.
3. A markup on the totals of Items 1 and 2 of this subsection 109.7.3 for home office overhead and profit of the Contractor, its Subcontractors and suppliers, and any lower tier Subcontractors or suppliers, with no mark-ups on mark-ups.
4. Cost for Equipment, based on Blue Book Rates or leased rates, as set forth in Section 109.7.5(C), or the Contractor’s Actual Costs.
5. Costs for extended job-site overhead.
6. Time.
7. Subcontractor quoted Work, as set forth below in Section 109.7.5 (F).”

109.7.5 Force Account Work

C. Equipment

Paragraph 2, delete sentence 1 which starts; “Equipment leased...”

Paragraph 6, change sentence 2 from “The Contractor may furnish...” to read “If requested by the Department, the Contractor will produce cost data to assist the Department in the establishment of such rental rate, including all records that are relevant to the Actual Costs including rental Receipts, acquisition costs, financing documents, lease Agreements, and maintenance and operational cost records.”

Add the following paragraph; “Equipment leased by the Contractor for Force Account Work and actually used on the Project will be paid for at the actual invoice amount plus 10% markup for administrative costs.”

Add the following section;

‘F. Subcontractor Quoted Work When accomplishing Force Account Work that utilizes Subcontractor quoted Work, the Contractor will be allowed a maximum markup of 5% for profit and overhead.’

SPECIAL PROVISION SECTION 401 HOT MIX ASPHALT PAVEMENT

401.18 Quality Control Method A & B Make the following change to paragraph a. QCP Administrator; in the final sentence, change “...certified as a Plant Technician or Paving Inspector...” to “...certified as a Quality Assurance Technologist...”

401.201 Method A Under a. Lot Size, add the following; ‘Each lot will be divided into a minimum of four sublots for mix properties and five sublots for percent TMD.’

SPECIAL PROVISION SECTION 402 PAVEMENT SMOOTHNESS

Add the following: “Projects to have their pavement smoothness analyzed in accordance with this Specification will be so noted in Special Provision 403 - Bituminous Box.”

‘402.02 Lot Size Lot size for smoothness will be 1000 lane-meters [3000 lane-feet]. A subplot will consist of 20 lane-meters [50 lane-feet]. Partial lots will be included in the previous lot if less than one-half the size of a normal lot. If greater than one-half the normal lot size, it will be tested as a separate lot.’

SPECIAL PROVISION SECTION 502 STRUCTURAL CONCRETE

502.0502 Quality Assurance Method A - Rejection by Resident Change the first sentence to read: “For an individual subplot with test results failing to meet the criteria in Table #1, or if the calculated pay factor for Air Content is less than 0.80.....”

502.0503 Quality Assurance Method B - Rejection by Resident Change the first sentence to read: “For material represented by a verification test with test results failing to meet the criteria in Table #1, the Department will.....”

502.0505 Resolution of Disputed Acceptance Test Results Combine the second and third sentence to read: “Circumstances may arise, however, where the Department may

SPECIAL PROVISION SECTION 504 REINFORCING STEEL

504.18 Plates for Fabricated Members Change the second paragraph, first sentence from: “...ASTM A 898/A 898 M...” to “...ASTM A 898/A 898 M or ASTM A 435/A 435 M as applicable and...”

SPECIAL PROVISION SECTION 535 PRECAST, PRESTRESSED CONCRETE SUPERSTRUCTURE

535.02 Materials Change “Steel Strand for Concrete Reinforcement” to “Steel Strand.” Add the following to the beginning of the third paragraph; “Concrete shall be Class P conforming to the requirements in this section. 28 day compressive strength shall be as stated on the plans. Coarse aggregate....”

535.26 Lateral Post-Tensioning Replace the first paragraph; “A final tension...” with “Overstressing strands for setting losses cannot be accomplished for chuck to chuck lengths of 7.6 m [25 ft] and less. In such instances, refer to the Plans for all materials and methods. Otherwise, post-tensioning shall be in accordance with PCI standards and shall provide the anchorage force noted in the Plans. The applied jacking force shall be no less than 100% of the design jacking force.”

SPECIAL PROVISION SECTION 604 MANHOLES, INLETS, AND CATCH BASINS

604.02 Materials Add the following:

“Tops and Traps	712.07
Corrugated Metal Units	712.08
Catch Basin and Manhole Steps	712.09”

SPECIAL PROVISION SECTION 615 LOAM

615.02 Materials Make the following change:

Organic Content

Percent by Volume

Humus

“5% - 10%”, as determined by Ignition Test

SPECIAL PROVISION SECTION 618
SEEDING

618.01 Description Change the first sentence to read as follows: “This work shall consist of furnishing and applying seed” Also remove “,and cellulose fiber mulch” from 618.01(a).

618.03 Rates of Application In 618.03(a), remove the last sentence and replace with the following: “These rates shall apply to Seeding Method 2, 3, and Crown Vetch.”

In 618.03(c) “1.8 kg [4 lb]/unit.” to “1.95 kg [4 lb]/unit.”

618.09 Construction Method In 618.09(a) 1, sentence two, replace “100 mm [4 in]” with “25 mm [1 in] (Method 1 areas) and 50 mm [2 in] (Method 2 areas)”

618.15 Temporary Seeding Change the Pay Unit from Unit to Kg [lb].

SPECIAL PROVISION SECTION 620
GEOTEXTILES

620.03 Placement Section (c)

Title: Replace “Non-woven” in title with “Erosion Control”.

First Paragraph: Replace first word “Non-woven” with “Woven monofilament”.

Second Paragraph: Replace second word “Non-woven” with “Erosion Control”.

620.07 Shipment, Storage, Protection and Repair of Fabric Section (a)

Replace the third sentence with the following: “Damaged geotextiles, as identified by the Resident, shall be repaired immediately.”

620.09 Basis of Payment

Pay Item 620.58: Replace “Non-woven” with “Erosion Control”

Pay Item 620.59: Replace “Non-woven” with “Erosion Control”

SPECIAL PROVISION SECTION 626
HIGHWAY SIGNING

626.034 Concrete Foundations Add to the following to the end of the second paragraph: “Pre-cast and cast-in-place foundations shall be warranted against leaning and corrosion for two years after the project is completed. If the lean is greater than 2 degrees from normal or the foundation is spalling within the first two years, the Contractor shall replace the foundation at no extra cost.”

SPECIAL PROVISION SECTION 637 DUST CONTROL

637.06 Basis of Payment Add the following after the second sentence of the third paragraph: “Failure by the Contractor to follow Standard Specification or Special Provision - Section 637 and/or the Contractor’s own Soil Erosion and Pollution Control Plan concerning Dust Control and/or the Contractor’s own Traffic Control Plan concerning Dust Control and/or visible evidence of excessive dust problems, as determined by the Resident, will result in a reduction in payment, computed by reducing the Lump Sum Total by 5% per occurrence per day. The Department’s Resident or any other representative of the Department reserves the right to suspend the work at any time and request a meeting to discuss violations and remedies. The Department shall not be held responsible for any delay in the work due to any suspension under this item. Additional penalties may also be assessed in accordance with Special Provision 652 - Work Zone Traffic Control and Standard Specification 656 - Temporary Soil Erosion and Water Pollution Control.”

SPECIAL PROVISION SECTION 639 ENGINEERING FACILITIES

639.04 Field Offices Change the forth to last paragraph from: “The Contractor shall provide a fully functional desktop copier...” to “....desktop copier/scanner...”

SPECIAL PROVISION SECTION 652 MAINTENANCE OF TRAFFIC

652.8.2 Other Items Replace the last paragraph with the following: “There will be no payment made under any 652 pay items after the expiration of the adjusted total contract time.”

SPECIAL PROVISION SECTION 656 TEMPORARY SOIL EROSION AND WATER POLLUTION CONTROL

656.5.1 If Pay Item 656.75 Provided Replace the second paragraph with the following: “Failure by the Contractor to follow Standard Specification or Special Provision - Section 656

and/or the Contractor's own Soil Erosion and Pollution Control Plan will result in a reduction in payment, computed by reducing the Lump Sum Total by 5% per occurrence per day. The Department's Resident or any other representative of the Department reserves the right to suspend the work at any time and request a meeting to discuss violations and remedies. The Department shall not be held responsible for any delay in the work due to any suspension under this item."

SPECIAL PROVISION SECTION 703 AGGREGATES

703.06 Aggregate for Base and Subbase Delete the first paragraph: "The material shall have..." and replace with "The material shall have a minimum degradation value of 15 as determined by Washington State DOT Test Method T113, Method of Test for Determination of Degradation Value (March 2002 version), except that the reported degradation value will be the result of testing a single specimen from that portion of a sample that passes the 12.5 mm [1/2 in] sieve and is retained on the 2.00 mm [No. 10] sieve, minus any reclaimed asphalt pavement used."

703.07 Aggregates for HMA Pavements Delete the forth paragraph: "The composite blend shall have..." and replace with "The composite blend, minus any reclaimed asphalt pavement used, shall have a Micro-Deval value of 18.0 or less as determined by AASHTO TP 58. In the event the material exceeds the Micro Deval limit, a Washington Degradation test shall be performed. The material shall be acceptable if it has a value of 30 or more as determined by Washington State DOT Test Method T 113, Method of Test for Determination of Degradation Value (March 2002 version) except that the reported degradation value will be the result of testing a single composite specimen from that portion of the sample that passes the 12.5mm [1/2 inch] sieve and is retained on the 2.00mm [No 10] sieve, minus any reclaimed asphalt pavement used."

703.22 Underdrain Backfill Material Change the first paragraph from "...for Underdrain Type B..." to "...for Underdrain Type B and C..."

SPECIAL PROVISION SECTION 709 REINFORCING STEEL AND WELDED STEEL WIRE FABIC

709.03 Steel Strand Change the second paragraph from "...shall be 12mm [1/2 inch] AASHTO M203M/M203 (ASTM A416/A416M)..." to "...shall be 15.24 mm [0.600 inch] diameter AASHTO M203 (ASTM A416)..."

SPECIAL PROVISION SECTION 712 MISCELLANEOUS HIGHWAY MATERIALS

Add the following:

“712.07 Tops, and Traps These metal units shall conform to the plan dimensions and to the following specification requirements for the designated materials.

Gray iron castings shall conform to the requirements of AASHTO M105, Class 30, unless otherwise designated.

Carbon steel castings shall conform to the requirements of AASHTO M103/M103M. Grade shall be 450-240 [65-35] unless otherwise designated.

Structural steel shall conform to the requirements of AASHTO M183/M183M or ASTM A283/A283M, Grade B or better. Galvanizing, where specified for these units, shall conform to the requirements of AASHTO M111.

712.08 Corrugated Metal Units The units shall conform to plan dimensions and the metal to AASHTO M36/M36M. Bituminous coating, when specified, shall conform to AASHTO M190 Type A.

712.09 Catch Basin and Manhole Steps Steps for catch basins and for manholes shall conform to ASTM C478M [ASTM C478], Section 13 for either of the following material:

- (a) Aluminum steps- ASTM B221M, [ASTM B211] Alloy 6061-T6 or 6005-T5.
- (b) Reinforced plastic steps Steel reinforcing bar with injection molded plastic coating copolymer polypropylene. Polypropylene shall conform to ASTM D 4101.

712.23 Flashing Lights Flashing Lights shall be power operated or battery operated as specified.

- (a) Power operated flashing lights shall consist of housing, adapters, lamps, sockets, reflectors, lens, hoods and other necessary equipment designed to give clearly visible signal indications within an angle of at least 45 degrees and from 3 to 90 m [10 to 300 ft] under all light and atmospheric conditions.

Two circuit flasher controllers with a two-circuit filter capable of providing alternate flashing operations at the rate of not less than 50 nor more than 60 flashes per minute shall be provided.

The lamps shall be 650 lumens, 120 volt traffic signal lamps with sockets constructed to properly focus and hold the lamp firmly in position.

The housing shall have a rotatable sun visor not less than 175 mm [7 in] in length designed to shield the lens.

Reflectors shall be of such design that light from a properly focused lamp will reflect the light rays parallel. Reflectors shall have a maximum diameter at the point of contact with the lens of approximately 200 mm [8 in].

The lens shall consist of a round one-piece convex amber material which, when mounted, shall have a visible diameter of approximately 200 mm [8 in]. They shall distribute light and not diffuse it. The distribution of the light shall be asymmetrical in a downward direction. The light distribution of the lens shall not be uniform, but shall consist of a small high intensity portion with narrow distribution for long distance throw and a larger low intensity portion with wide distribution for short distance throw. Lenses shall be marked to indicate the top and bottom of the lens.

(b) Battery operated flashing lights shall be self-illuminated by an electric lamp behind the lens. These lights shall also be externally illuminated by reflex-reflective elements built into the lens to enable it to be seen by reflex-reflection of the light from the headlights of oncoming traffic. The batteries must be entirely enclosed in a case. A locking device must secure the case. The light shall have a flash rate of not less than 50 nor more than 60 flashes per minute from minus 30 °C [minus 20 °F] to plus 65 °C [plus 150 °F]. The light shall have an on time of not less than 10 percent of the flash cycle. The light beam projected upon a surface perpendicular to the axis of the light beam shall produce a lighted rectangular projection whose minimum horizontal dimension shall be 5 degrees each side of the horizontal axis. The effective intensity shall not have an initial value greater than 15.0 candelas or drop below 4.0 candelas during the first 336 hours of continuous flashing. The illuminated lens shall appear to be uniformly bright over its entire illuminated surface when viewed from any point within an angle of 9 degrees each side of the vertical axis and 5 degrees each side of the horizontal axis. The lens shall not be less than 175 mm [7 in] in diameter including a reflex-reflector ring of 13 mm [½ in] minimum width around the periphery. The lens shall be yellow in color and have a minimum relative luminous transmittance of 0.440 with a luminance of 2854° Kelvin. The lens shall be one-piece construction. The lens material shall be plastic and meet the luminous transmission requirements of this specification. The case containing the batteries and circuitry shall be constructed of a material capable of withstanding abuse equal to or greater than 1.21 mm thick steel [No. 18 U.S. Standard Gage Steel]. The housing and the lens frame, if of metal shall be properly cleaned, degreased and pretreated to promote adhesion. It shall be given one or more coats of enamel which, when dry shall completely obscure the metal. The enamel coating shall be of such quality that when the coated case is struck a light blow with a sharp tool, the paint will not chip or crack and if scratched with a knife will not powder. The case shall be so constructed and closed as to exclude moisture that would affect the proper operation of light. The case shall have a

weep hole to allow the escape of moisture from condensation. Photoelectric controls, if provided, shall keep the light operating whenever the ambient light falls below 215 lx [20 foot candles]. Each light shall be plainly marked as to the manufacturer's name and model number.

If required by the Resident, certification as to conformance to these specifications shall be furnished based on results of tests made by an independent testing laboratory. All lights are subject to random inspection and testing. All necessary random samples shall be provided to the Resident upon request without cost to the Department. All such samples shall be returned to the Contractor upon completion of the tests.

712.32 Copper Tubing Copper tubing and fittings shall conform to the requirements of ASTM B88M Type A [ASTM B88, Type K] or better.

712.33 Non-metallic Pipe, Flexible Non-metallic pipe and pipe fittings shall be acceptable flexible pipe manufactured from virgin polyethylene polymer suitable for transmitting liquids intended for human or animal consumption.

712.34 Non-metallic Pipe, Rigid Non-metallic pipe shall be Schedule 40 polyvinylchloride (PVC) that meets the requirement of ASTM D1785. Fittings shall be of the same material.

712.341 Metallic Pipe Metallic pipe shall be ANSI, Standard B36.10, Schedule 40 steel pipe conforming to the requirements of ASTM A53 Types E or S, Grade B. End plates shall be steel conforming to ASTM A36/A36M.

Both the sleeve and end plates shall be hot dip galvanized. Pipe sleeve splices shall be welded splices with full penetration weld before galvanizing.

712.35 Epoxy Resin Epoxy resin for grouting or sealing shall consist of a mineral filled thixotropic, flexible epoxy resin having a pot life of approximately one hour at 10°C [50°F]. The grout shall be an approved product suitable for cementing steel dowels into the preformed holes of curb inlets and adjacent curbing. The sealant shall be an approved product, light gray in color and suitable for coating the surface.

712.36 Bituminous Curb The asphalt cement for bituminous curb shall be of the grade required for the wearing course, or shall be Viscosity Grade AC-20 meeting the current requirements of Subsection 702.01 Asphalt Cement. The aggregate shall conform to the requirements of Subsection 703.07. The coarse aggregate portion retained on the 2.36 mm [No. 8] sieve may be either crushed rock or crushed gravel.

The mineral constituents of the bituminous mixture shall be sized and graded and combined in a composite blend that will produce a stable durable curbing with an acceptable texture.

Bituminous material for curb shall meet the requirements of Section 403 - Hot Bituminous Pavement.

712.37 Precast Concrete Slab Portland cement concrete for precast slabs shall meet the requirements of Section 502 - Structural Concrete, Class A.

The slabs shall be precast to the dimension shown on the plans and cross section and in accordance with the Standard Detail plans for Concrete Sidewalk Slab. The surface shall be finished with a float finish in accordance with Subsection 502.14(c). Lift devices of sufficient strength to hold the slab while suspended from cables shall be cast into the top or back of the slab.

712.38 Stone Slab Stone slabs shall be of granite from an acceptable source, hard, durable, predominantly gray in color, free from seams which impair the structural integrity and be of smooth splitting character. Natural color variations characteristic of the deposit will be permitted. Exposed surfaces shall be free from drill holes or indications of drill holes. The granite slabs in any one section of backslope must be all the same finish.

The granite slabs shall be scabble dressed or sawed to an approximately true plane having no projections or depressions over 13 mm [½ in] under a 600 mm [2 ft] straightedge or over 25 mm [1 in] under a 1200 mm [4 ft] straightedge. The arris at the intersection of the top surface and exposed front face shall be pitched so that the arris line is uniform throughout the length of the installed slabs. The sides shall be square to the exposed face unless the slabs are to be set on a radius or other special condition which requires that the joints be cut to fit, but in any case shall be so finished that when the stones are placed side by side no space more than 20 mm [¾ in] shall show in the joint for the full exposed height.

Liftpin holes in all sides will be allowed except on the exposed face.

SPECIAL PROVISION SECTION 717 ROADSIDE IMPROVEMENT MATERIAL

717.05 Mulch Binder. Change the third sentence to read as follows:

“Paper fiber mulch may be used as a binder at the rate of 2.3 kg/unit [5 lb/unit].”

Town: Alton, Rt. 16
Project: STP-1020(70)X, PIN 10207.00
Date: Monday, March 01, 2004

**SPECIAL PROVISION
SECTIONS 104
UTILITIES**

MEETING

A pre-construction utility meeting, as defined in Article 104.4.6 of the Standard Specifications, is hereby called for.

GENERAL

These Special Provisions outline the arrangements that have been made by the Department for utility work to be undertaken in conjunction with this project. The following list identifies all known utilities having facilities presently located within the limits of this project or intending to install facilities during project construction

Overview:

Utility/Railroad	Aerial	Underground
Bangor-Hydro Electric Company	X	
Mid-Maine Telecom	X	X

Temporary utility adjustments are not anticipated at this time. If temporary relocation becomes necessary, sufficient time will need to be allowed prior to the construction for all required temporary relocation.

All utility crossings over highways will provide not less than 18 feet vertical clearance over existing ground in cut or over finished grade in fill, during construction of this project.

Any times and dates mentioned are estimates only and are dependent upon favorable weather, working conditions, and freedom from emergencies. The Contractor shall have no claim against the Department if they are exceeded.

Utility working days are Monday through Friday, conditions permitting. Times are estimated on the basis of a single crew for each utility.

In all cases the Utilities shall be advised well in advance (generally three weeks) before work, dependent upon other work to be done by the Contractor, in any particular area, is to be commenced by them.

AERIAL

Bangor Hydro-Electric Company has several poles to relocate as part of this job. The pole relocation at Sta. 297+714 Rt. will need a spot cut to allow Bangor Hydro Electric Company. The contractor shall prioritize this work as to facilitate a prompt relocate by Bangor Hydro-Electric. Above and beyond the poles specified in this document, all above ground utility locations (poles, guys, etc.) will

Special Provisions - Utilities**Alton, Rt. 16, PIN 10207.00****Tuesday, August 26, 2003**

be reviewed for compliance with the Department's Above Ground Pole Policy following the completion of the paving operation. Any above ground utility locations not meeting the Department's Above Ground Pole Policy will require relocation to the proper offset.

Utility/Railroad	Pole Set	Transfer wire or Install new wire	Remove Poles	Estimated Working Days
Bangor-Hydro Electric	X	X	X	15
Mid-Maine Telecom		X		10
			Total:	50

Pole List:

Existing Pole #	Existing Station	Left/Right		Offset	Proposed Station	Left/Right		Offset	Comments
		LT	RT			LT	RT		
No. #	295+873		X	5.8	295+873		X	6.2	Behind GR in fill section
No. #	295+931		X	6.8			X	6.8	
No. #	295+990		X		295+990		X	6.6	
No. #	296+465		X	5.6	296+465		X	6.2	
No. #	296+536		X	5.6	296+536		X	6.2	
No. #	296+609		X	5.6	296+609		X	6.2	
No. #	296+686		X	5.3	296+686		X	6.2	
									Begin Cut
No. #	297+628		X	6.3					OK
No. #	297+714		X	5.40	297+714		X	6.2	8' Cut
No. #	297+775		X	6.1	297+775		X	6.2	

UNDERGROUND

Mid-Maine Telecom has buried cable in the shoulder the entire length of the project. Contractor shall notify the telephone companies at least three (3) days prior to any guardrail relocation or ditching operations to allow the utility to determine the cable locations in that area. The contact for **Mid-Maine Telecom** is **Ron Bragg 992-9932**. After the initial layout, it is the Contractor's responsibility to maintain the location markings. Any damage to the buried cable caused by the Contractor during construction shall be repaired by the utility at the contractor's expense.

UTILITY SIGNING

Any utility working within the construction limits of this project shall ensure that the traveling public is adequately protected at all times. All work areas shall be signed, lighted, and traffic flaggers employed as determined by field conditions. All traffic controls shall be in accordance with the latest

Special Provisions - Utilities

Alton, Rt. 16, PIN 10207.00

Tuesday, August 26, 2003

edition of the Manual on Uniform Traffic Control Devices for Streets and Highways, as issued by the Federal Highway Administration.

DIG SAFE

The Contractor shall be responsible for determining the presence of underground utility facilities prior to commencing any excavation work and shall notify utilities of proposed excavation in accordance with M.R.S.A. Title 23 §3360-A, Maine "Dig Safe" System.

MAINTAINING UTILITY LOCATION MARKINGS

The Contractor will be responsible for maintaining the buried utility location markings following the initial locating by the appropriate utility or their designated representative.

THE CONTRACTOR SHALL PLAN AND CONDUCT HIS WORK ACCORDINGLY.

The following utilities are known to be located on this project:

Bangor Hydro-Electric Company	Bruce Smith	973-2507
Mid-Maine Telecom	Ron Bragg	992-9932

April 6, 1999

SPECIAL PROVISION
SECTION 105
CONTROL OF WORK
(Cooperation Between Contractors)

It is hereby brought to the Contractor's attention that the Department has awarded and plans to award contracts adjacent to the limits of this contract, which may be in progress simultaneously.

The Contractor shall cooperate with other Contractors at all times and provide project access as necessary and as directed by the Resident.

Alton
PIN #: 10207.00
Date: 3/17/04

SPECIAL PROVISION
SECTION 105
General Scope of Work
(Environmental Requirements)

Instream Work shall not be allowed between the dates of 10/2 and 7/14.
(Instream work is allowed from 7/15 to 10/1.)

Stream Name(s) with Station #s: Ten Mile Brook

Special Conditions: All proper BMP's will be used, Fish passage must be maintained

Instream work consists of any activity conducted below normal high water mark.

All activities are prohibited (including placement and removal of cofferdams) below normal high water during the instream work window restriction, except for the following:

- Work within a sealed and dewatered cofferdam. Maintenance pumping within a sealed cofferdam is also allowed.

No construction activity, whether temporary or permanent, is allowed that completely blocks a river, stream, or brook without providing downstream flow.

The contractor shall abide by all permits and conditions.

- Dredge Material (See MDOT Standard Specifications § 101.2) is regulated as a Special Waste. Fifty cubic yards or less of Dredge Material **Beneficially Used in the area adjacent to and draining into the dredged water body** is exempt from Beneficial Use Permits. The Contractor shall ensure that Dredge Material is placed into the fill areas specified by MDOT. No more than the fifty cubic yards (38 cubic meters) of Dredge Material may be excavated without authorization from the Resident. Any Dredge Material not Beneficial Used (excess Dredge Material) shall be disposed of at a landfill licensed by the Maine Department of Environmental Protection to accept Special Waste. The Contractor shall be responsible for making all necessary arrangements for dewatering and proper disposal of the Dredge Material, including any additional laboratory testing, in accordance with the landfill's license. The Contractor shall provide documentation to the Resident that any such Dredge Material was disposed of as specified.

SPECIAL PROVISION
SECTION 107
SCHEDULING OF WORK

Replace Section 107.4.2 with the following:

"107.4.2 Schedule of Work Required Within 21 Days of Contract Execution and before beginning any on-site activities, the Contractor shall provide the Department with its Schedule of Work. The Contractor shall plan the Work, including the activity of Subcontractors, vendors, and suppliers, such that all Work will be performed in Substantial Conformity with its Schedule of Work. The Schedule must include sufficient time for the Department to perform its functions as indicated in this Contract, including QA inspection and testing, approval of the Contractor's TCP, SEWPCP and QCP, and review of Working Drawings.

At a minimum, the Schedule of Work shall include a bar chart which shows the major Work activities, milestones, durations, and a timeline. Milestones to be included in the schedule include: (A) start of Work, (B) beginning and ending of planned Work suspensions, (C) Completion of Physical Work, and (D) Completion. If the Contractor Plans to Complete the Work before the specified Completion date, the Schedule shall so indicate.

Any restrictions that affect the Schedule of Work such as paving restrictions or In-Stream Work windows must be charted with the related activities to demonstrate that the Schedule of Work complies with the Contract.

The Department will review the Schedule of Work and provide comments to the Contractor within 20 days of receipt of the schedule. The Contractor will make the requested changes to the schedule and issue the finalized version to the Department."

SPECIAL PROVISION

SECTION 107

TIME

(Contract Time)

1. The contractor will be allowed to commence work at any time as long as the Soil Erosion and Water Pollution Plan, Traffic Control Plan and all other applicable plans have been submitted and approved. The Office trailer must be in-place and approved.
2. Completion date is **October 30, 2004**.
3. For every weekday not worked after operations begin, the contractor will be charged liquidated damages per calendar day (excluding inclement weather days) at the rate stated in 107.7.2

SPECIAL PROVISION

SECTION 108

RECYCLED ASPHALT PAVEMENT WITH BITUMINOUS ADDITIVE PERFORMANCE GRADED BINDER PRICE ADJUSTMENT

Price adjustments will be based on the variance in costs for the performance graded binder component of recycled asphalt pavement with bituminous additive. They will be determined as follows:

Performance Graded Asphalt Binder The quantity of asphalt cement will be determined by taking the quantity of recycled asphalt pavement with bituminous additive (**62,100 M²**) and multiplying by (**0.0023 for item #310.23**) times the difference in price in excess of 5 percent between the base price and the period price of asphalt cement. Adjustments will be made upward or downward, as prices increase or decrease.

Recycled Asphalt Pavement with Bituminous Additive The quantity of recycled asphalt pavement with bituminous additive will be determined from field measurements and shown on the progress estimate for each pay period.

Base Price The base price of performance graded binder to be used is the price per standard ton current with the bid opening date. This price is determined by using the average N.E. Barge Price, FOB, as listed in the Asphalt Weekly Monitor.

Period Price The period price of performance graded binder will be determined by the Department by using the average N.E. Barge Price, FOB, listed in the Asphalt Weekly Monitor current with the pay period ending date of the progress estimate.

SPECIAL PROVISION
SECTION 310
PLANT MIXED RECYCLED ASPHALT PAVEMENT

310.01 Description This work shall consist of the removal of all bituminous pavement from the existing roadway, hauling the bituminous pavement to an approved location, and processing as per Section 310.020. The gravel base of the existing roadway shall be regarded and compacted to the tolerances shown on the typicals, or as directed by the Resident.

All plant mixed recycled asphalt pavement shall be placed in one or more courses on an approved base and in accordance with these specifications, and in reasonably close conformity with the lines, grades and thicknesses indicated on the plans, or as established by the Resident. Excess recycled material not used in the PMRAP process will become the property and responsibility of the contractor.

MATERIALS

310.020 Composition of Mixture The mixture shall be composed as directed in the job mix formula. The recycled asphalt pavement shall be processed by the Contractor so all material will be no larger than 37.5 mm [1.5 in] and stockpiled so as to minimize segregation. The stockpile shall be free of any materials not generally considered to be asphalt pavement. If additional material is required, the material will be supplied by the State or acquired from the Contractor through the Contract Modification process.

A job mix formula shall be furnished by the Department establishing the percentage of emulsified asphalt cement, Portland Cement, aggregate, and water to be used in the mixture. The JMF additive proportions will be verified by taking a second recycled material sample once the stockpiles have been constructed.

Emulsion, water, aggregate and Portland Cement shall be added in percentage by weight and verified by tank checks done in accordance with the minimum quality control frequencies. Cement additive may be done in dry form or introduced as a cement slurry.

310.021 Emulsified Asphalt The emulsified asphalt shall be grade MS-2, MS-4, CSS-1, or HFMS-2 meeting the requirements of Section 702.04 - Emulsified Asphalt.

310.022 Portland Cement Portland Cement shall be Type I or II meeting the requirements of AASHTO M85.

310.023 Water Water shall be clean and free from deleterious concentrations of acids, alkalis, salts or other organic or chemical substances.

310.024 New Aggregate New aggregate, if required by the contract or job mix, shall meet the requirements of Section 411.02 - Untreated Aggregate Surface Course.

EQUIPMENT

310.030 Mixing Plant The mixing plant shall be of sufficient capacity and coordinated to adequately handle the proposed construction. Either a continuous pugmill mixer or a continuous drum type mixing plant shall be used. If a

drum mixing plant is used it shall meet the requirements of Section 401.07. The mixing plant shall be capable of producing a uniform mixture meeting the requirements of the job mix formula.

310.031 Hauling Equipment Trucks used for hauling the mixture shall meet the requirements of Section 401.08.

310.032 Bituminous Pavers Pavers shall meet the requirements of Section 401.09.

310.033 Rollers Rollers shall meet the requirements of Section 401.10.

CONSTRUCTION REQUIREMENTS

310.040 Mixing The recycled asphalt pavement shall be delivered to the mixer at a temperature of not less than 10°C [50°F]. The emulsified asphalt shall meet the mixing temperature requirements listed in Section 702.05 - Application Temperatures. Recycled pavement and emulsified asphalt, and cement shall be proportioned and the mixing time set to produce a mixture in which uniform distribution of the emulsified asphalt and coating of the recycled pavement is obtained.

If a drum type mixing plant is used, the recycled asphalt pavement may be heated prior to being mixed with the emulsified asphalt to a temperature not to exceed 90°C [195°F].

Following mixing, the recycled asphalt pavement material shall be stockpiled and incorporated into the work. The material shall not be stockpiled for longer than 24 hours.

310.041 Weather Limitations The plant mixed recycled asphalt pavement shall be performed when:

- a. PM-RAP operations will be allowed between May 15th and September 15th inclusive in Zone 1 - Areas north of US Route 2 from Gilead to Bangor and north of Route 9 from Bangor to Calais. PM-RAP will be allowed between May 1st and September 30th inclusive in Zone 2 - Areas south of Zone 1 including the US Route 2 and Route 9 boundaries.
- b. The atmospheric temperature, as determined by an approved thermometer placed in the shade at the recycling location, is 10°C [50°F] and rising.
- c. When there is no standing water on the surface.
- d. During generally dry conditions, or when weather conditions are such that proper pulverizing, adding, mixing, and curing can be obtained using proper procedures, and when compaction can be accomplished as determined by the Resident.
- e. When the surface is not frozen and when overnight temperatures are expected to be above 0°C [32°F].

310.042 Spreading and Finishing The mixture shall be spread and finished in accordance with Section 401.15. Total layer thickness greater than 100 mm [4 in] will be placed in 2 lifts.

310.043 Compaction Compaction of the mixture shall be in accordance with Section 401.16. Rolling may be delayed to avoid lateral displacement as directed by the Resident. See also Section 310.051.

310.044 Joints Joints shall be constructed in accordance with Section 401.17.

310.045 Surface Tolerances The surface tolerances shall be as specified in Section 401.101, except that the maximum allowable variation shall be 10 mm [$\frac{3}{8}$ in]. The surface tolerance in existing gravel areas covered by PMRAP, with no additional gravel, shall be ± 10 mm [$\frac{3}{8}$ in].

TESTING REQUIREMENTS

310.050 Quality Control The Contractor shall operate in accordance with the approved Quality Control Plan (QCP) to assure a product meeting the contract requirements. The QCP shall meet the requirements of Section 106.6 - Acceptance and this Section. The Contractor shall not begin recycling operations until the Department approves the QCP in writing.

Prior to performing any recycling process, the Department and the Contractor shall hold a Pre-recycle conference to discuss the recycling schedule, type and amount of equipment to be used, sequence of operations, and traffic control. A copy of the QC random numbers to be used on the project shall be provided to the Resident. All field and plant supervisors including the responsible onsite recycling process supervisor shall attend this meeting.

The QCP shall address any items that affect the quality of the Recycling Process including, but not limited to, the following:

- a. JMF(s).
- b. Mixing details, pugmill type, production rates, material processing.
- c. Make and type of paver(s).
- d. Make and type of rollers including weight, weight per inch of steel wheels, and average contact pressure for pneumatic tired rollers.
- e. Testing Plan.
- f. Transportation including process for ensuring that truck bodies are clean and free of debris or contamination that could adversely affect the finished product, type of release agent used (if required)
- g. Laydown operations including procedures for mix design modification, avoiding recycling and curing in inclement weather, material yield monitoring, methods to ensure that segregation is minimized, longitudinal joint construction, procedures to determine the maximum rolling and placing speeds based on field quality control, and achieving the best possible smoothness.
- h. Methods for protecting the finished product from damage and procedures for any necessary corrective action.
- i. Method of grade checks.
- j. Examples of Quality Control forms.
- k. Name, responsibilities, and qualifications of the Responsible onsite Recycling Supervisor experienced and knowledgeable with the process.
- l. Method for calibration/verification of density gauge.
- m. A note that all testing will be done in accordance with AASHTO and MDOT/ACM procedures.
- n. Stockpile procedures including method of moisture control.

The Project Superintendent shall be named in the QCP, and the responsibilities for successful implementation of the QCP shall be outlined.

The Contractor shall sample, test, and evaluate the PMRAP process in accordance with the following procedures and minimum frequencies:

MINIMUM QUALITY CONTROL FREQUENCIES

Test or Action	Frequency	Test Method
Density	1 per 300 m [1000 ft] / lane	ASTM D 2950
Air Temperature	4 per day at even intervals	
Surface Temperature	At the beginning and end of each days operation	
Yield of all materials (Both the daily yield and yield since last test)	4 per day at even intervals	

The Contractor shall submit all QC test reports and summaries in writing, signed by the appropriate technician, and present them to the Department's onsite representative by 1:00 P.M. on the next working day, except when otherwise noted in the QCP due to local restrictions. The Contractor shall make all test results, including randomly sampled densities, available to the Department onsite.

The Contractor shall cease recycling operations whenever one of the following occurs:

- The computed yield differs from the approved Job Mix Formula by 10% or more.
- The Contractor fails to follow the approved QCP.
- The Contractor fails to achieve 98% density after corrective action has been taken.

Recycling operations shall not resume until the Contractor and the Department agree on the corrective action to be taken.

310.051 Test strip The contractor shall assemble all items of equipment for the recycling operation on the first day of the recycling work. The Contractor shall construct a test strip for the project at a location approved by the Resident. The test strip section is required to:

- Demonstrate that the equipment and processes can produce recycled layers to meet the requirements specified in these special provisions;
- Determine the effect on the grading of the recycled material by varying the forward speed of the paving machine; and;
- Determine the sequence and manner of rolling necessary to obtain the compaction requirements and establish a target TMD. The Contractor and the Department will calibrate their respective gauges at this time.

The test strip shall be at least 100 m [300 ft] in length of a full lane-width (or a half-road width).

Full PMRAP production will not begin until an acceptable test strip has been constructed. If a test strip fails to meet the requirements of this specification, the Contractor will be required to repair or replace the test strip to the

satisfaction of the Resident. Any repairs, replacement, or duplication of the test strip will be at the Contractor's expense.

Quality Assurance density testing of the recycled material will be performed by the Department using the nuclear method. After the test strip has been placed, it will be rolled as directed until the nuclear density readings show an increase in density of less than 16 kg/m³ [1 pcf] for the final four roller passes. The test strip density will be used as the target density for the recycled material. The remaining PMRAP material shall be compacted to a minimum density of 98% of the target density as determined in the control section.

ACCEPTANCE TEST FREQUENCY

Property	Frequency	Test Method
In-place Density	1 per 600 m [2000 ft] / lane	ASTM D 2950

310.052 Repairs Repairs and maintenance for the PMRAP layers, during and after the curing period, resulting from damage caused by traffic, weather or environmental conditions, or caused by the Contractor's operations or equipment, shall be completed at no additional cost to the Department.

Low areas will be repaired using a hot mix asphalt shim course. Areas up to 25mm [1 in] high can be repaired by milling or shimming with hot mix asphalt. Areas higher than 25mm [1 in] will be repaired using a hot mix asphalt shim. All repair work will be done with the Resident's approval at the Contractor's expense.

310.06 Curing No new hot mix asphalt pavement or additional layers of PM-RAP shall be placed on the recycled asphalt pavement until a curing period of (4) four days has elapsed. The curing period starts once the PM-RAP has been placed in the roadway. When weather conditions are unfavorable, the curing period may be extended by the Resident.

310.07 Method of Measurement Plant Mixed Recycled Asphalt Pavement shall be measured by the square meter [square yard].

310.08 Basis of Payment The accepted quantity of Plant Mixed Recycled Asphalt Pavement will be paid for at the contract unit price per square meter [square yard], complete in-place which price will be full compensation for furnishing all equipment and labor for removing existing pavement, regrading and compacting existing gravel base, processing, mixing, testing, placing, and compacting, excess material relocation, and for all incidentals necessary to complete the work.

Payments will be made under:

<u>Pay Item</u>	<u>Pay Unit</u>
310.23 - 75mm [3 in] Plant Mixed Recycled Asphalt Pavement	Square Meter [yd ²]
310.24 - 100mm [4 in] Plant Mixed Recycled Asphalt Pavement	Square Meter [yd ²]
310.25 - 125mm [5 in] Plant Mixed Recycled Asphalt Pavement	Square Meter [yd ²]
310.26 - 150mm [6 in] Plant Mixed Recycled Asphalt Pavement	Square Meter [yd ²]

Special Provision
SECTION 310
PLANT MIXED RECYCLED ASPHALT PAVEMENT
(Excess material to State)

310.01 Description Delete the last sentence and add the following; “Excess pavement millings not used in the PM-RAP process will become the property of the State. All removed pavement will be processed in accordance with section 310.020 and hauled to an approved State stockpile located at The MDOT maintenance lot on route 16. All processing, hauling, and stockpiling will be considered incidental to the Contract.

SPECIAL PROVISION
SECTION 310
Plant Mix Recycled Asphalt Pavement
Mix Design

The Plant Mix Recycled Asphalt Pavement on this project will be treated with the following material proportions:

Emulsion	3.25%
Water	3.0% – 6.0%
Portland cement (Type I or II)	1.25 %

The optimum moisture content for compaction shall be determined by the Department using samples obtained from the pulverized material prior to addition of the foamed asphalt, by means of AASHTO T 180, Method D.

A contract modification will be executed if percentages change from the requirements above for added emulsion or Portland cement by more than 0.10%. Positive and negative price adjustments will be made. The price adjustment will be based upon receipted bills for materials delivered the project site. If a price adjustment is warranted, the contractor will supply the Department with all receipted bills for emulsion and Portland cement for the entire project. Adjustments in water content exceeding the initial targets shall not be paid for directly, but shall be incidental.

SPECIAL PROVISION
SECTION 403
HOT MIX ASPHALT OVERLAY

Desc. of Course	Grad. Design	Item Number	Bit Cont. % of Mix	Total Thick	No. Of Layers	Comp. Notes
<u>PMRAP Treated Areas</u>						
<u>Mainline Travelway and Shoulders</u>						
Wearing	9.5mm	403.210	N/A	30mm	1	4,7
Base	12.5mm	403.213	N/A	45mm	1/more	4,7
<u>Shoulder Widening for Guardrail</u>						
Wearing	9.5mm	403.210	N/A	30mm	1	4,7
Base	12.5mm	403.213	N/A	45mm	1/more	4,7
<u>Approach, Side Roads</u>						
Wearing	9.5mm	403.210	N/A	30mm	1	4,7
Base	12.5mm	403.213	N/A	45mm	1/more	4,7
<u>Drives, Islands, Misc.</u>						
Wearing	9.5mm	403.209	N/A	30-50mm	1/more	2,3,9,10,13

COMPLEMENTARY NOTES

2. The density requirements are waived.
3. The design traffic level for mix placed shall be <0.3 million ESALS.
4. The design traffic level for mix placed shall be 0.3 to <3 million ESALS.
7. Section 106.6 Acceptance, (1) Method A.
9. Section 106.6 Acceptance, (2) Method C.
10. A **“FINE”** 9.5 mm mix with a gradation above or through the restricted zone shall be used for this item.
13. A mixture meeting the requirements of section 703.09 Grading ‘D’, with a minimum PGAB content of 6%, and the limits of Special Provision 401, Table 9 (Drives and Sidewalks) for PGAB content and gradation may be substituted for this item. A job mix formula shall be submitted to the department for approval.

Tack Coat

A tack coat of emulsified asphalt, RS-1 or HFMS-1, Item #409.15 shall be applied to any existing pavement **and PMRAP surface** at a rate of approximately 0.08 L/m², and on milled pavement approximately 0.2 L/m², prior to placing a new course. A fog coat of emulsified asphalt shall be applied between shim / intermediate course and the surface course, at a rate not to exceed 0.08 L/m². Tack used between layers of pavement will be paid for at the contract unit price for Item 409.15 Bituminous Tack Coat.

SPECIAL PROVISION
SECTION 652
MAINTENANCE OF TRAFFIC
(Traffic Control)

652.7 Method of Measurement. This entire Subsection is revised to read:
Traffic Control Supervisor, furnishing, installation, and maintenance of all traffic control devices will be measured as one **lump sum** for all work authorized and performed.

652.8 Basis of Payment. This entire Subsection is revised to read:
Traffic Control will be paid for at the contract **lump sum** price. Payment will be full compensation for the Traffic Control Supervisor, approach signs, work area signs, drums, cones, panel markers, barricades, arrow boards etc. and maintenance thereof including the setting up and taking down of lane closures as many times as necessary shall be considered part of the lump sum price.

Maintenance of signs includes: replacing devices damaged, lost, or stolen, and cleaning and moving as many times as necessary throughout the life of the contract, regardless whether the work areas or projects are geographically separated or not separated.

The Lump Sum will be payable in installments as follows: 5% of the Lump Sum once the approach signing is complete and approved, with the 95% balance to be paid as the work progresses at a rate proportional to the percentage completion of the Contract.

Failure by the contractor to follow the Contracts 652 Special Provisions and/or The Manual on Uniform Traffic Control Devices (MUTCD) and/or The Contractors own Traffic Control Plan will result in a reduction in payment, computed by reducing The Lump Sum Total by 5% per occurrence. The Departments Resident Engineer or any other representative of The Department reserves the right to suspend the work at any time and request a meeting to discuss violations and remedies. The Department shall not be held responsible for any delay in the work due to any suspension under this item.

All other requirements under the Standard Specifications Section 652 will be a part of the lump sum item.

There will be no extra payment for this pay item after the expiration of contract time.

Payment will be made under:

<u>Pay Item</u>	<u>Pay Unit</u>
652.39 Work Zone Traffic Control	Lump Sum

SPECIAL PROVISION
SECTION 652
MAINTENANCE OF TRAFFIC

Approaches Approach signing shall include the following signs as a minimum. Field conditions may warrant the use of additional signs as determined by the Resident.

Road Work Next x Miles
Road Work 500 Feet
End Road Work

Work Area At each work site, signs and channelizing devices shall be used as directed by the Resident. Signs include:

Road Work xxxx¹
One Lane Road Ahead
Flagger Sign

Other typical signs include:

Be Prepared to Stop
Low Shoulder
Bump
Pavement Ends

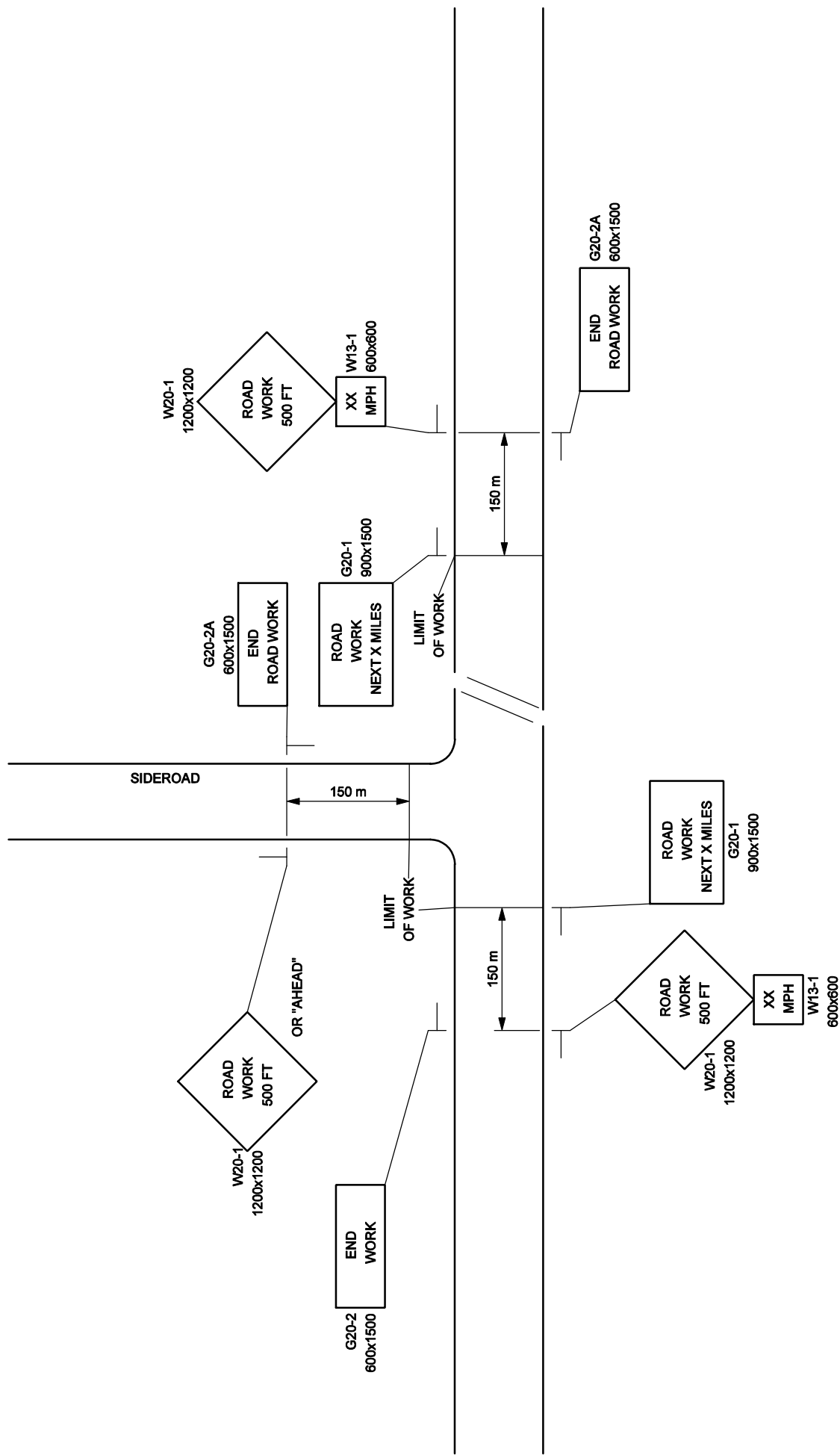
The above lists of Approach signs and Work Area signs are representative of the contract requirements. Other sign legends may be required.

The Contractor shall conduct their operations in such a manner that the roadway will not be restricted to one lane for more than 800 m [2,500 ft] at each work area. Where more than one work area restricts traffic to one lane operation, these work areas shall be separated by at least 1.6 km [1 mile] of two way operation.

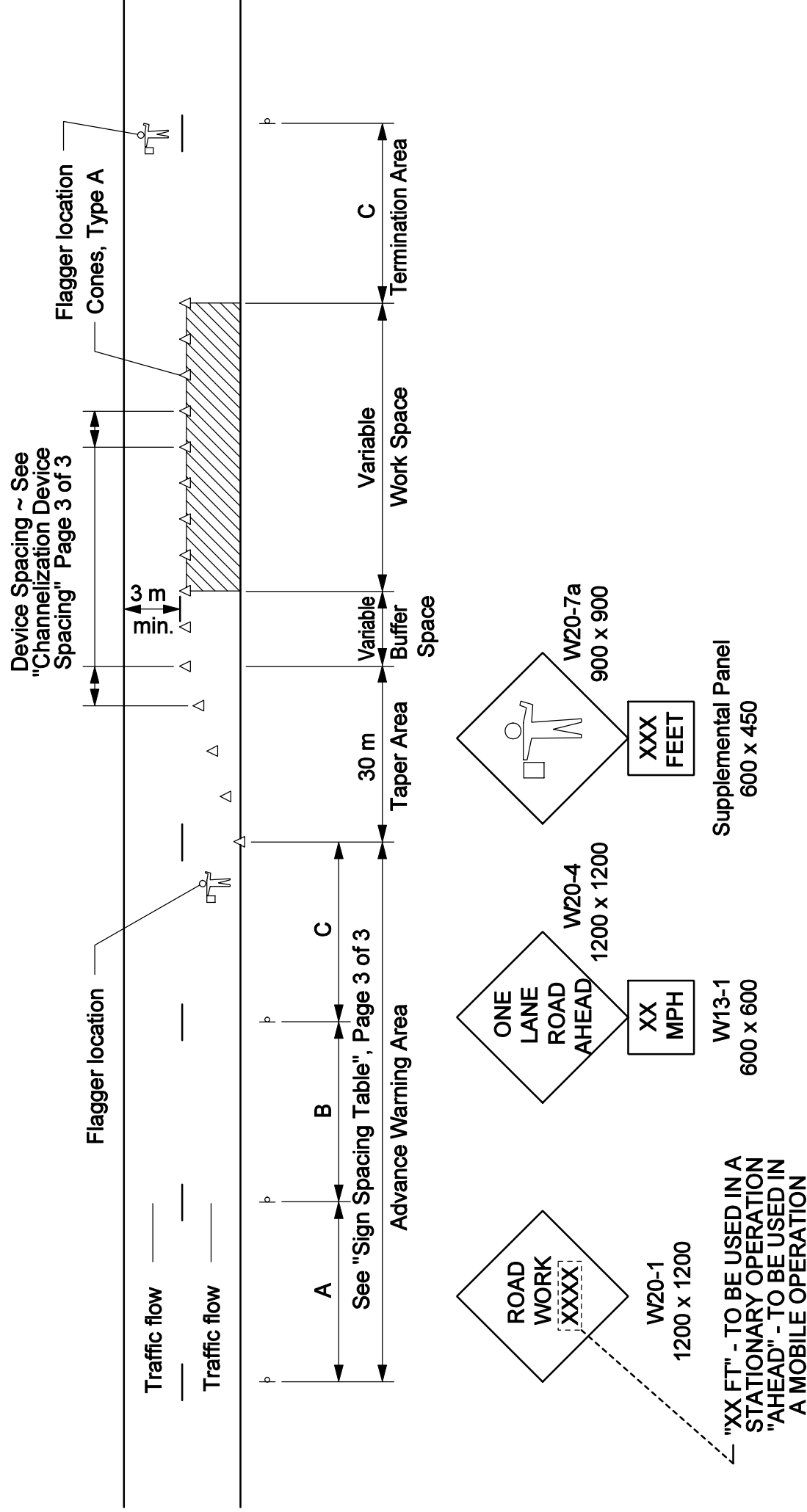
Temporary Centerline A temporary centerline shall be placed each day on all new pavement to be used by traffic. The temporary centerline, when specified of reflectorized traffic paint, shall conform to the standard marking patterns used for permanent markings.

Failure to apply a temporary centerline daily will result in suspension of paving until temporary markers are applied to all previously placed pavement.

¹ "Road Work Ahead" to be used in mobile operations and "Road Work xx ft" to be used in stationary operations as directed by the Resident.



TYPICAL -- PROJECT APPROACH SIGNING -- TWO WAY TRAFFIC



TYPICAL APPLICATION: TWO - WAY, TWO LANE ROADWAY,
CLOSING ONE LANE USING FLAGGERS

* Formulas for L are as follows:

For speed limits of 40 mph (60 km/h) or less:

$$L = \frac{WS^2}{60} \quad (L = \frac{WS^2}{155})$$

For speed limits of 45 mph (70 km/h) or greater:

$$L = WS \quad (L = \frac{WS}{1.6})$$

* Formulas for L are as follows:

A minimum of 5 channelization devices shall be used in the taper.

TYPE OF TAPER	TAPER LENGTH (L)*
Merging Taper	at least L
Shifting Taper	at least 0.5L
Shoulder Taper	at least 0.33L
One-Lane, Two-Way Traffic Taper	100 ft (30 m) maximum
Downstream Taper	100 ft (30 m) per lane

CHANNELIZATION DEVICE SPACING

The spacing of channelization devices shall not exceed a distance equal to 1.0 times the speed limit in mph when used for taper channelization, and a distance in feet of 2.0 times the speed limit in mph when used for tangent channelization.

GENERAL NOTES;

1. Final placement of signs and devices may be changed to fit field conditions as approved by the Resident.

SIGN SPACING TABLE			
Road Type	Distance Between Signs**		
	A	B	C
Urban 30 mph (50 km/h) or less	100 (30)	100 (30)	100 (30)
Urban 35 mph (55 km/h) and greater	350 (100)	350 (100)	350 (100)
Rural	500 (150)	500 (150)	500 (150)
Expressway / Urban Parkway	2,640 (800)	1,500 (450)	1000 (300)

**Distances are shown in feet (meters).

SUGGESTED BUFFER ZONE LENGTHS

Speed (mph)	Length (feet)	Speed (mph)	Length (feet)
20	115	40	325
25	155	45	360
30	200	50	425
35	250	55	495

**SPECIAL PROVISION
SECTION 656**

Temporary Soil Erosion and Water Pollution Control

The following is added to Section 656 regarding Project Specific Information and Requirements. All references to the Maine Department of Transportation Best Management Practices for Erosion and Sediment Control (a.k.a. Best Management Practices manual or BMP Manual) are a reference to the latest revision of said manual. The "Table of Contents" of the latest version is dated "1/19/00" (available at <http://www.state.me.us/mdot/mainhtml/bmp/bmpjan2000.pdf>.)

Procedures specified shall be according to the BMP Manual unless stated otherwise.

Delete the last sentence of Section 656.4.4, which reads, "After Final Acceptance of the project, the Contractor must submit the log to the Department which will become the property of the Department."

Any and all references to "bark mulch" or "composted bark mix" shall be a reference to "Erosion Control Mix" in accordance with *Standard Specification, Section 619 - Mulch*.

Project Specific Information and Requirements

The following information and requirements apply specifically to this Project. The temporary soil erosion and water pollution control measures associated with this work shall be addressed in the SEWPCP.

- 1) This project is located within the "Pushaw Lake" watershed, which has been classified as a priority water body as defined by the MDOT BMP Manual, due to the proximity to the resource, strict erosion and sediment controls are necessary.
- 2) Newly disturbed earth shall be mulched by the end of each workday. Mulch shall be maintained on a daily basis.
- 3) If **water is flowing within the drainage system, the water shall be diverted to a stable area or conduit and work shall be conducted in the dry.** The Contractor's plan shall address when and where the diversions will be necessary.
- 4) Dust control items other than those under *Standard Specification, Section 637 – Dust Control*, if applicable, shall be included in the plan.
- 5) Permanent slope stabilization measures shall be applied within one week of the last soil disturbance.

**SPECIAL PROVISION
SECTION 656**

Temporary Soil Erosion and Water Pollution Control

- 6) Permanent seeding shall be done in accordance with *Standard Specification, Section 618 - Seeding* unless the Contract states otherwise.
- 7) Culvert inlet and outlet protection shall be installed within 48 hours of culvert installation, or prior to a storm event, whichever is sooner.
- 8) **DRAINAGE WORK SHALL NOT COMMENCE UNTIL AFTER APRIL 1ST OF ANY YEAR DUE TO HIGH GROUNDWATER LEVELS AND NUMEROUS INTERMITTENT STREAMS.** This date shall only be adjustable upon approval of both the Construction Manager and a representative from the Water Resources Unit.
- 9) All disturbed ditches shall be stabilized by the end of each workday. Stabilization shall be maintained on a daily basis.
- 10) Erosion control blanket shall be installed in the bottoms of all ditches except where a stone lining is planned. Seed shall be applied prior to the placement of the blanket.
- 11) If check dams are used, they shall be constructed of stone in accordance with BMP Manual, Section
- 12) After November 1 the Contractor shall use winter stabilization methods, such as Erosion Control Mix as specified in *Standard Specification, Section 619 - Mulch*. If required, spring procedures for permanent stabilization shall also be described in the plan. Use of this product for over-winter temporary erosion control will be incidental to the contract and be paid for as part of Pay Item 656.75.
- 13) Stream flow shall be maintained at all times. Grout from the post-tensioning socket sealing operation and fresh concrete shall not be allowed to contact the stream. Clean out of concrete delivery trucks and the washing of tools shall be addressed in the SEWPCP.
- The SEWPCP shall describe the containment method for removal of the existing abutments, including installation of cofferdams and dewatering procedures.
- A cofferdam sedimentation basin is required if cofferdams are used. The basin shall be located in an upland area where the water can settle and seep into the ground or be released slowly to the resource in a manner that will not cause erosion. The location of such a cofferdam sedimentation basin shall be addressed in the SEWPCP.

Permits & Cultural Resources Unit

PIN #: 10207.00

Location: Alton, Rte 16

Permit Member: Laurie Rowe

Photographs ☐

Database/Projex ☒

Package to ENV Coordinator: 3/17/04

☒ **HISTORIC AND CULTURAL RESOURCES**

MHPC Historic Resources	N/A <input type="checkbox"/>	Applicable <input checked="" type="checkbox"/>	Approved <input checked="" type="checkbox"/>
MHPC Archeological Resources	N/A <input type="checkbox"/>	Applicable <input checked="" type="checkbox"/>	Approved <input checked="" type="checkbox"/>
Advisory Council on Hist Preservation	N/A <input checked="" type="checkbox"/>	Applicable <input type="checkbox"/>	Approved <input type="checkbox"/>
NPS Recordation	N/A <input checked="" type="checkbox"/>	Applicable <input type="checkbox"/>	Approved <input type="checkbox"/>
State Recordation	N/A <input checked="" type="checkbox"/>	Applicable <input type="checkbox"/>	Approved <input type="checkbox"/>

☒ **4(f) and 6(f)**

Section 4(f)	N/A <input checked="" type="checkbox"/>	Applicable <input type="checkbox"/>	Approved <input type="checkbox"/>
LAWCON 6(f)	N/A <input checked="" type="checkbox"/>	Applicable <input type="checkbox"/>	Approved <input type="checkbox"/>

☒ **Maine Department of Environmental Protection (MDEP) Site Location of Development**

N/A ☒ Applicable ☐ Approved ☐

☒ **Local Zoning, Title 30-A, Section 4325-6.**

Is the project something other than the highway and bridge system, such as a maintenance lot, building/parking facility?

Yes ☐ No ☒. If no, the project is exempt.

If yes, continue. Does the town in which the project is located have a comprehensive plan consistent with the Growth Management Program? Yes ☐ No ☐. If no, the project is exempt.

If yes, local zoning ordinances and/or permits are needed. Approved ☐

☒ **Maine Department of Inland Fisheries and Wildlife (MDIFW) Essential Habitat**

Eagle Nest	N/A <input checked="" type="checkbox"/>	Applicable <input type="checkbox"/>	Approved <input type="checkbox"/>
Piping Plover	N/A <input checked="" type="checkbox"/>	Applicable <input type="checkbox"/>	Approved <input type="checkbox"/>
Roseate Tern	N/A <input checked="" type="checkbox"/>	Applicable <input type="checkbox"/>	Approved <input type="checkbox"/>

☒ **United States Fish and Wildlife Service (USFWS), Migratory Bird Act**

N/A ☒ Applicable ☐

☒ **Maine Department of Conservation/ Public Lands, Submerged Land Lease**

N/A ☒ Applicable ☐

☒ **Land Use Regulation Commission (LURC)** ☒ **Not Applicable**

No permit	<input type="checkbox"/>	
Notice	<input type="checkbox"/>	Approved <input type="checkbox"/>
Permit	<input type="checkbox"/>	Approved <input type="checkbox"/>

☒ **Maine Department of Environmental Protection (MDEP), Natural Resource Protection Act**

No permit required	<input type="checkbox"/>	
Exempt	<input type="checkbox"/>	(Must use erosion and sediment control and not block fish passage.)
PBR	<input checked="" type="checkbox"/>	Approved <input checked="" type="checkbox"/>
Tier 1	<input type="checkbox"/>	Approved <input type="checkbox"/>
Tier 2	<input type="checkbox"/>	Approved <input type="checkbox"/>
Tier 3	<input type="checkbox"/>	Approved <input type="checkbox"/>

☒ **Army Corps of Engineers (ACOE), Section 10 of the Rivers and Harbors Act and Section 404 of the Clean Water Act.**

No permit required	<input type="checkbox"/>	
Category 1-NR	<input checked="" type="checkbox"/>	Approved <input checked="" type="checkbox"/>
Category 2	<input type="checkbox"/>	Approved <input type="checkbox"/>
Category 3	<input type="checkbox"/>	Approved <input type="checkbox"/>

☒ **IN-WATER TIMING RESTRICTIONS: 105 Special Provision** ☒ n/a ☐

Dates instream work is allowed: 7/15 - 10/1

☒ **Special Provision 656, Erosion Control Plan**

* Boxes marked in red indicate items that are attached and need to be placed in the contract by the Project Manager.

DEPARTMENT OF ENVIRONMENTAL PROTECTION (DEP)
PERMIT BY RULE NOTIFICATION FORM
(For use with DEP Regulation, Chapter 305)

■ MDOT PIN: 10207.00

Name of Applicant: State of Maine Department of Transportation Name of Contact: David Gardner
Mailing Address: 16 Station State House Town/City: Augusta State: Me. Zip Code: 04330-0016
Daytime Telephone #: (207)-624-3100 Name of Wetland, Water Body or Stream: Unnamed

Detailed Directions to Site: Project is on Rte 16 beginning 0.85 miles southerly of Lagrange-Alton town line extending southerly 4.63 miles.

Town/City: Lagrange Map #: N/A Lot #: N/A County: Penobscot

Description of Project: Highway overlay involving culvert, guardrail, slope and ditch maintenance and repairs. The project will be performed in accordance with erosion control measures conforming with the latest versions of the *State of Maine Department of Transportation Standard Specifications for Highways and Bridges* and the *Department of Transportation's Best Management Practices for Erosion and Sediment Control*.

Part of a larger project? ☐ Yes ☒ No

(CHECK ONE) This project... ☒ does ☐ does not ...involve work below mean low water.

I am filing notice of my intent to carry out work which meets the requirements for Permit By Rule (PBR) under DEP Regulation, Chapter 305. I have a copy of PBR Sections checked below. I have read and will comply with all of the standards.

- | | | |
|---|---|--|
| <input type="checkbox"/> Sec. (2) Soil Disturbance | <input type="checkbox"/> Sec. (8) Shoreline stabilization | <input type="checkbox"/> Sec. (14) Piers, Wharves & Pilings |
| <input type="checkbox"/> Sec. (3) Intake Pipes | <input type="checkbox"/> Sec. (9) Utility Crossing | <input type="checkbox"/> Sec. (15) Public Boat Ramps |
| <input type="checkbox"/> Sec. (4) Replacement of Structures | <input type="checkbox"/> Sec. (10) Stream Crossing | <input type="checkbox"/> Sec. (16) Coastal Sand Dune Projects |
| <input type="checkbox"/> Sec. (5) REPEALED | <input checked="" type="checkbox"/> Sec. (11) State Transport. Facilities | <input checked="" type="checkbox"/> Sec. (17) Transfers/Permit Extension |
| <input type="checkbox"/> Sec. (6) Movement of Rocks or Vegetation | <input type="checkbox"/> Sec. (12) Restoration of Natural Areas | <input type="checkbox"/> Sec. (18) Maintenance Dredging |
| <input type="checkbox"/> Sec. (7) Outfall Pipes | <input type="checkbox"/> Sec. (13) F&W Creation/Enhance/Water Quality Improvement | |

I authorize staff of the Departments of Environmental Protection, Inland Fisheries & Wildlife, and Marine Resources to access the project site for the purpose of determining compliance with the rules. I also understand that **this permit is not valid until approved by the Department or 14 days after receipt by the Department, whichever is less.**

I have attached all of the following required submittals. **NOTIFICATION FORMS CANNOT BE ACCEPTED WITHOUT THE NECESSARY ATTACHMENTS:**

- A \$50 (non-refundable) payment shall be done by internal billing.
- **Attach** a U.S.G.S. topo map or Maine Atlas & Gazetteer map with the project site clearly marked.
- ☐ **Attach** photographs showing existing site conditions (unless not required under standards).

Signature of Applicant: _____

John E. Dority, Chief Engineer

Date: _____

03/19/04

Keep the bottom copy as a record of permit. Send the form with attachments via certified mail to the Maine Dept. of Environmental Protection at the appropriate regional office listed below. The DEP will send a copy to the Town Office as evidence of the DEP's receipt of notification. No further authorization by DEP will be issued after receipt of notice. Permits are valid for two years. **Work carried out in violation of any standard is subject to enforcement action.**

AUGUSTA DEP STATE HOUSE STATION 17 AUGUSTA, ME 04333-0017 (207)287-2111 PORTLAND DEP 312 CANCO ROAD PORTLAND, ME 04103 (207)822-6300 BANGOR DEP 106 HOGAN ROAD BANGOR, ME 04401 (207)941-4570 PRESQUE ISLE DEP 1235 CENTRAL DRIVE PRESQUE ISLE, ME 04769 (207)764-0477

OFFICE USE ONLY
PBR # FP

Ck.#

Date

Staff

Acc. Date

Staff
Def. Date

After Photos

Chapter 305: PERMIT BY RULE Section 11
State Transportation Facilities

- 1. Introduction.** A "permit by rule" or "PBR", when approved by the Department of Environmental Protection (DEP), is an approval for an activity that requires a permit under the Natural Resources Protection Act (NRPA). Only those activities described in this chapter may proceed under the PBR process. A PBR activity will not significantly affect the environment if carried out in accordance with this chapter, and generally has less of an impact on the environment than an activity requiring an individual permit. A PBR satisfies the Natural Resources Protection Act (NRPA) permit requirement and Water Quality Certification requirement.

If a proposed activity is not described in this chapter, or will not be conducted in accordance with the standards of this chapter, the applicant must obtain an individual permit prior to beginning the activity.

- A. Location of activity.** The location of an activity may affect whether an activity qualifies for PBR, and whether review by the Department of Inland Fisheries and Wildlife is required.

- (1) Type of resource. For some types of activities, the availability of a PBR is affected by the type of natural resource in or adjacent to which the activity is proposed. For example, an applicant proposing an activity consisting of "Movement of rocks or vegetation" may receive a PBR only if the activity will take place in a great pond, river, stream or brook. Limitations concerning the location of activities are addressed in the "Applicability" provision in each section of this chapter.
- (2) Essential habitat. Essential habitats include areas critical to the survival of threatened and endangered species such as the bald eagle, least tern, roseate tern, and piping plover. If the activity is located in essential habitat, such as near an eagle nesting site, a PBR is only available if the applicant obtains written approval from the Department of Inland Fisheries and Wildlife (IF&W). This approval from IF&W must be submitted to the DEP with the PBR notification form, and the applicant must follow any conditions stated in the IF&W approval.

NOTE: Maps showing areas of essential habitat are available from the Department of Inland Fisheries and Wildlife regional headquarters, municipal offices, the Land Use Regulation Commission (for unorganized territories) and DEP regional offices. If the activity is located in essential habitat, IF&W must be contacted to request and obtain a "certification of review and approval".

- B. Notification.** The applicant must file notice of the activity with the DEP prior to beginning work on the activity. The notification must be on a form provided by the DEP and must include any submissions required in this chapter. The applicant must keep a copy to serve as the permit.

The notification form must be sent to the DEP by certified mail (return receipt requested), or hand delivered to the DEP and date stamped by the department.

C. Effective period

- (1) Beginning of period. The PBR becomes effective 14 calendar days after the DEP receives the notification form, unless the DEP approves or denies the PBR prior to that date. If the DEP does not speak with or write to the applicant within this 14 day period regarding the PBR notification, the applicant may proceed to carry out the activity.

There are three exceptions regarding the effective date of an approved PBR:

- (a) Activities listed in Section 10 (Stream crossings) occurring in association with forest management are exempt from the 14 day waiting period.
- (b) Activities listed in Section 2 (Soil disturbance) and Section 10 (Stream crossings) performed or supervised by individuals currently certified in erosion control practices by the DEP are exempt from the 14 day waiting period. To be certified in erosion control practices, an individual must successfully complete all course requirements of the Voluntary Contractor Certification Program administered by the DEP's Nonpoint Source Training and Resource Center.
- (c) Activities that are part of a larger project requiring a permit under the Site Location of Development or the Storm Water Management Acts may not proceed until any required permit under those laws is obtained.

NOTE: Activities that are part of a larger project may require other permits from the DEP also. These other laws may prohibit the start of construction of any part of the project unless a permit under that law is obtained. In these cases, while not a violation of this rule, starting work on a PBR approved activity would be a violation of those other applicable laws.

- (2) End of period. The PBR is generally effective for 2 years from the date of approval, except that a PBR for "Replacement of structures" under Section 4 is effective for 3 years.

NOTE: Activities that qualify under this chapter may need to meet other local, state and federal requirements. Examples -- (1) If an activity extends below the low water line of a lake, coastal wetland or international boundary water, the applicant should contact the Bureau of Parks and Lands (287-3061) concerning possible lease or easement requirements, or (2) If an activity will involve work below the mean high water line in navigable waters of the United States, the applicant should contact the Army Corps of Engineers (623-8367).

D. Discretionary authority. Notwithstanding compliance with the PBR applicability requirements and standards set forth in this chapter, the DEP may require an individual permit application to be filed in any case where credible evidence indicates that the activity:

- (1) May violate the standards of the NRPA (38 M.R.S.A. Section 480-D);
- (2) Could lead to significant environmental impacts, including cumulative impacts; or
- (3) Could adversely impact a resource of special concern.

If an individual permit is required pursuant to this subsection, the DEP shall notify the applicant in writing within the 14 calendar day waiting period described in sub-section (C) above. When the DEP notifies an applicant that an individual permit is required, no work may be conducted unless and until the individual permit is obtained.

E. Violations. A violation of law occurs when a person, or his or her agent, performs or causes to be performed any activity subject to the NRPA without first obtaining a permit from the DEP, or acts contrary to the provisions of a permit. The person, his or her agent, or both, may be held

responsible for the violation. Commonly, the "person" is the landowner, and the "agent" is the contractor carrying out the activity. A violation occurs when:

- (1) An activity occurs that is not allowed under PBR, whether or not a PBR notification form has been filed with and/or approved by the DEP;
- (2) An activity occurs that is allowed under PBR, but a PBR for the activity has not become effective prior to the beginning of the activity; or
- (3) An activity occurs that is allowed under PBR and a PBR for the activity is in effect, but the standards specified in this chapter are not met.

See the "applicability" provision under each activity for rules concerning what activities are allowed under PBR. A PBR is only valid for the person listed on the notification form, or for his or her agent.

Each day that a violation occurs or continues is considered a separate offense. Violations are subject to criminal penalties and civil penalties of not less than \$100 nor more than \$10,000 for each day of that violation (38 M.R.S.A. Section 349).

NOTE: A local Code Enforcement Officer (CEO) may take enforcement action for a violation of the Natural Resources Protection Act if he or she is authorized to represent a municipality in District Court, and he or she has been certified as familiar with court procedures, 30-A M.R.S.A. Section 4452(7).

Chapter 305 Section 11**State transportation facilities****A. Applicability**

- (1) This section applies to the maintenance, repair, reconstruction, rehabilitation, replacement or minor construction of a State Transportation Facility carried out by, or under the authority of, the Maine Department of Transportation or the Maine Turnpike Authority, including any testing or preconstruction engineering, and associated technical support services.
- (2) This section does not apply to an activity within a coastal sand dune system.

NOTE: The construction of a transportation facility other than roads and associated facilities may be subject to the Storm Water Management Law, 38 M.R.S.A. Section 420-D.

B. Standards

- (1) Photographs of the area to be altered by the activity must be taken before work on the site begins. The photographs must be kept on file and be made available at the request of the DEP.
- (2) The activity must be reviewed by the Department of Inland Fisheries and Wildlife, the Department of Marine Resources, the Atlantic Salmon Authority, and the DEP's Division of Environmental Assessment prior to the notification being filed with the DEP. The activity must be performed according to any recommendations from these authorities.
- (3) The activity must be performed in accordance with erosion control measures conforming with the State of Maine Department of Transportation Standard Specifications for Highways and Bridges Revision of April 1995 and with the Department of Transportation's Best Management Practices for Erosion and Sediment Control, September 1997.

NOTE: Guidance on the use of erosion control best management practices can be obtained from the on site Construction Manager.

- (4) Alignment changes may not exceed a distance of 200 feet between the old and new center lines in any natural resource.
- (5) The activity may not alter more than 300 feet of shoreline (both shores added together) within a mile stretch of any river, stream or brook, including any bridge width or length of culvert.
- (6) The activity may not alter more than 150 feet of shoreline (both shores added together) within a mile stretch of any outstanding river segment identified in 38 M.R.S.A. 480-P, including any bridge width or length of culvert.
- (7) The activity must minimize wetland intrusion. The activity is exempt from the provisions of Chapter 310, the Wetland Protection Rules, if the activity alters less than 15,000 square feet of natural resources per mile of roadway (centerline measurement) provided that the following impacts are not exceeded within the 15,000 square foot area:

- (a) 1,000 square feet of coastal wetland consisting of salt tolerant vegetation or shellfish habitat; or
- (b) 5,000 square feet of coastal wetland not containing salt tolerant vegetation or shellfish habitat; or
- (c) 1,000 square feet of a great pond.

All other activities must be performed in compliance with all sections of Chapter 310, the Wetland Protection Rules, except 310.2(C), 5(A), 9(1), 9(B) and 9(C).

- (8) The activity may not permanently block any fish passage in any watercourse containing fish. The applicant must improve passage beyond what restriction may already exist unless the Department of Inland Fisheries and Wildlife, the Department of Marine Resources, the Atlantic Salmon Authority and the DEP's Division of Environmental Assessment concur that the improvement is not necessary.
- (9) Rocks may not be removed from below the normal high water line of any coastal wetland, freshwater wetland, great pond, river, stream or brook except to the minimum extent necessary for completion of work within the limits of construction.
- (10) If work is performed in a river, stream or brook that is less than three feet deep at the time and location of the activity, with the exception of culvert installation, the applicant must divert flow away from the activity while work is in progress.
 - (a) Diversion may be accomplished by the use of stable, inert material. No more than two thirds (2/3) of stream width may be diverted at one time.
 - (b) Any material used to divert water flow must be completely removed upon completion of the activity, and the stream bottom must be restored to its original condition.
 - (c) A pump may be operated, where necessary, for a temporary diversion. The pump outlet must be located and operated such that erosion or the discharge of sediment to the water is prevented.

NOTE: Guidance on the appropriate location of a diversion and materials which should be used for a stream diversion can be obtained from the on site Construction Manager.

- (11) Wheeled or tracked equipment may not operate in the water. Equipment operating on the shore may reach into the water with a bucket or similar extension. Equipment may cross streams on rock, gravel or ledge bottom.
- (12) All wheeled or tracked equipment that must travel or work in a vegetated wetland area must travel and work on mats or platforms.
- (13) Any debris or excavated material must be stockpiled either outside the wetland or on mats or platforms. Hay bales or silt fence must be used, where necessary, to prevent sedimentation. Any debris generated during the activity must be prevented from washing downstream and must be removed from the wetland or water body. Disposal of debris must be in conformance with the Maine Hazardous Waste, Septage and Solid Waste Management Act, 38 M.R.S.A. Section 1301 et seq.

- (14) Work below the normal high water line of a great pond, river, stream or brook must be done at low water except for emergency work or work agreed to by the resource agencies listed in paragraph 2 above. Measures, such as a silt boom or staked fencing, must be employed to reduce and isolate turbidity.
- (15) Perimeter controls must be installed before the work starts. Disturbance of natural resources beyond the construction limits shown on the plans is not allowed under this rule.

NOTE: Guidance on the location of construction limits can be obtained from the on site Construction Manager.

- (16) The use of untreated lumber is preferred. Lumber pressure treated with chromated copper arsenate (CCA) may be used, provided it is cured on dry land in a manner that exposes all surfaces to the air for a period of at least 21 days prior to construction. Wood treated with creosote or pentachlorophenol may not be used where it will contact water.
- (17) A temporary road for equipment access must be constructed of crushed stone, blasted ledge, or similar materials that will not cause sedimentation or restrict fish passage. Such roads must be completely removed at the completion of the activity. In addition, any such temporary roads which are in rivers, streams or brooks, must allow for a passage of stormwater flows associated with a 10-year storm.
- (18) Soil may not be disturbed during any period when soils are saturated due to rain or snow melt, except as necessary to protect work in progress or as required for bridge maintenance activities. Areas where soils are saturated (i.e. water drips from the soil when squeezed by hand, or the soil is capable of being rolled into a rod 1/8th inch in diameter that does not crumble) must be immediately mulched if they are disturbed.
- (19) Disturbed soil must be protected within one week from the time it was last actively worked, and prior to any storm event, using temporary or permanent measures such as the placement of riprap, sod, mulch, erosion control blankets, or other comparable measures.
- (20) Hay bale or straw mulch, where used, must be applied at a rate of at least one bale per 500 square feet (1 to 2 tons per acre).
- (21) If mulch is likely to be moved because of steep slopes or wind exposure, it must be anchored with netting, peg and twine, binder or other suitable method and must be maintained until a catch of vegetation is established over the entire disturbed area.
- (22) In addition to the placement of riprap, sod, erosion control blankets or mulch, additional steps must be taken where necessary to prevent sedimentation of the water. Evidence of sedimentation includes visible sheet, rill or gully erosion, discoloration of water by suspended particles and/or slumping of banks. Silt fences, staked hay bales and other sedimentation control measures, where planned for, must be in place prior to the commencement of an activity, but must also be installed whenever necessary to prevent erosion and sedimentation.

NOTE: Guidance on the location and proper installation of erosion control measures can be obtained from the on site Construction Manager.

- (23) Temporary erosion control measures must be maintained and inspected weekly until the site is permanently stabilized with vegetation or other permanent control measures. Erosion control measures must also be inspected immediately prior to and following storms.
- (24) Permanent erosion control measures protecting all disturbed areas must be implemented within 30 days from the time the areas were last actively worked, or for fall and winter activities by the following June 15, except where precluded by the type of activity (e.g. riprap, road surfaces, etc.). The permanent erosion control measures must be maintained.
- (25) The applicant shall immediately take appropriate measures to prevent erosion or sedimentation from occurring or to correct any existing problems, regardless of the time of year.
- (26) Non-native species may not be planted in restored areas.
- (27) Disposal of debris must be in conformance with Maine Hazardous Waste, Septage and Solid Waste Management Act, 38 M.R.S.A. Sections 1301 et seq.
- (28) Disturbance of vegetation must be avoided, if possible. Where vegetation is disturbed outside of the area covered by any road or structure construction, it must be reestablished immediately upon completion of the activity and must be maintained.
- (29) A vegetated area at least 25 feet wide must be established and maintained between any new stormwater outfall structure and the high water line of any open water body. A velocity reducing structure must be constructed at the outlet of the stormwater outfall that will create sheet flow of stormwater, and prevent erosion of soil within the vegetated buffer. If the 25 foot vegetated buffer is not practicable, the applicant must explain the reason for a lesser setback in writing. Approval from the DEP must be in writing and any recommendations must be incorporated into the activity.

C. Definitions. The following terms, as used in this chapter, have the following meanings, unless the context indicates otherwise:

- (1) Diversion. A rerouting of a river, stream or brook to a location outside of its established channel.
- (2) Fill. a. (verb) To put into or upon, supply to, or allow to enter a water body or wetland any earth, rock, gravel, sand, silt, clay, peat, or debris; b. (noun) Material, other than structures, placed in or immediately adjacent to a wetland or water body.
- (3) Floodplain wetlands. Freshwater wetlands that are inundated with flood water during a 100-year flood event based on flood insurance maps produced by the Federal Emergency Agency or other site specific information.
- (4) Riprap. Rocks that are fit into place, usually without mortar, on a slope as defined in the State of Maine, Department of Transportation, Standard Specifications for Highway and Bridges, revision of April 1995.

Permit No: GP-39

Effective Date: Sept. 29, 2000
Expiration Date: Sept. 29, 2005

Applicant: General Public, State of Maine

**DEPARTMENT OF THE ARMY
PROGRAMMATIC GENERAL PERMIT
STATE OF MAINE**

The New England District of the U.S. Army Corps of Engineers hereby issues a programmatic general permit (PGP) that expedites review of minimal impact work in coastal and inland waters and wetlands within the State of Maine. Activities with minimal impacts, as specified by the terms and conditions of this general permit and on the attached DEFINITION OF CATEGORIES sheets, are either non-reporting (provided required local and state permits are received), or are reporting, to be screened by the Corps and Federal Resource Agencies for applicability under the general permit. This general permit does not affect the Corps individual permit review process or activities exempt from Corps jurisdiction.

Activities Covered: work and structures that are located in, or that affect, navigable waters of the United States (regulated by the Corps under Section 10 of the Rivers and Harbors Act of 1899) and the discharge of dredged or fill material into waters of the United States (regulated by the Corps under Section 404 of the Clean Water Act), and the transportation of dredged material for the purpose of disposal in the ocean (regulated by the Corps under Section 103 of the Marine Protection, Research and Sanctuaries Act).

PROCEDURES:

A. State Approvals

For projects authorized pursuant to this general permit that are also regulated by the State of Maine, the following state approvals are also required and must be obtained in order for this general permit authorization to be valid (applicants are responsible for ensuring that all required state permits and approval have been obtained):

- (a) Maine Department of Environmental Protection (DEP): Natural Resources Protection Act permit, including permit-by-rule and general permit authorizations; Site Location and Development Act permit; and Maine Waterway Development and Conservation Act.
- (b) Maine Department of Conservation: Land Use Regulation Commission (LURC) permit.
- (c) Maine Department of Marine Resources: Lease.
- (d) Bureau of Public Lands, Submerged Lands: Lease.

Note that projects not regulated by the State of Maine (e.g., seasonal floats or moorings) may still be authorized by this general permit.

B. Corps Authorizations: Category I (Non-Reporting)

Work in Maine subject to Corps jurisdiction that meets the definition of Category I on the attached DEFINITION OF CATEGORIES sheets and that meets all of this permit's other conditions, does not require separate application to the Corps of Engineers. If the State or the Corps does not contact the applicant for PBRs and Tier One permits during the State's Tier One 30-day review period, Corps approval may be assumed and the project may proceed. Refer to the Procedures Section at Paragraph E below for additional information regarding screening.

Note that the review thresholds under Category I apply to single and complete projects i only (see special condition 5). **Also note that Category I does not apply to projects occurring in a component of, or within 0.25 miles up and downstream of the main stem or tributaries of a river segment of the National Wild and Scenic River System** (see condition 11, and page 9 for the listed rivers in Maine).

There are also restrictions on other national lands or concerns, which must be met in order for projects to be eligible for authorization under this PGP. Refer to special conditions 6-13 under Paragraph F below.

Work that is not regulated by the State of Maine, but that is subject to Corps jurisdiction, is eligible for Corps authorization under this PGP in accordance with the review thresholds and conditions contained herein.

Although Category I projects are non-reporting, the Corps reserves the right to require screening or an individual permit review if there are concerns for the aquatic environment or any other factor of the public interest (see special condition 4 on Discretionary Authority). The Corps review or State/Federal screening process may also result in project modification, mitigation or other special conditions necessary to minimize impacts and protect the aquatic environment as a requirement for PGP approval.

C. Corps Authorization: Category II (Reporting - requiring screening) APPLICATION PROCEDURES

For projects that do not meet the terms of Category I (see DEFINITION OF CATEGORIES sheets), the Corps, State, and Federal Resource Agencies will conduct joint screening meetings to review applications. If projects are concurrently regulated by the DEP or LURC, applicants do not need to submit separate applications to the Corps. For projects not regulated by DEP or LURC, applicants must submit an application to the Corps Maine Project Office for a case-by-case determination of eligibility under this general permit (Category II). **Category II projects may not proceed until written notification is received from the Corps.**

Category II projects which occur in a component of, or within 0.25 mile up or downstream of the main stem or tributaries of a river segment of the National Wild and Scenic River System, will be coordinated with the National Park Service (see special condition 11, and page 9 for listed rivers in Maine).

There are also restrictions on other national lands or concerns, which must be met in order for projects to be eligible for authorization under this PGP. Refer to special conditions 6-14 under Paragraph E below.

Category II applicants shall submit a copy of their application materials to the Maine Historic Preservation Commission and/or applicable Indian tribe(s) at the same time, or before, they apply to the DEP, LURC, or the Corps so that the project can be reviewed for the presence of historic/archaeological resources in the project area that may be affected by the proposed work. **Applications to the DEP or the Corps should include information to indicate that this has been done (applicant's statement or copy of cover letter to Maine Historic Preservation Commission and/or Indian tribe(s)).**

The Corps may require additional information on a case-by-case basis as follows:

- (a) purpose of project;
- (b) 8 1/2" by 11" plan views of the entire property including property lines and project limits with existing and proposed conditions (**legible, reproducible plans required**);
- (c) wetland delineation for the site, information on the basis of the delineation, and calculations of waterway and wetland impact areas (see special condition 2);
- (d) typical cross-section views of all wetland and waterway fill areas and wetland replication areas;
- (e) delineation of submerged aquatic vegetation, e.g., eel grass beds, in tidal waters;
- (f) area, type and source of fill material to be discharged into waters and wetlands, including the volume of fill below ordinary high water in inland waters and below the high tide line in coastal waters;
- (g) mean low, mean high water and high tide elevations in navigable waters;
- (h) limits of any Federal navigation project in the vicinity and State Plane coordinates for the limits of the proposed work closest to the Federal project;
- (i) on-site alternatives analysis (contact Corps for guidance);
- (j) identify and describe potential impacts to Essential Fish Habitat (contact Corps for guidance);
- (k) for dredging projects, include:
 - 1) the volume of material and area in square feet to be dredged below mean high water,
 - 2) existing and proposed water depths,
 - 3) type of dredging equipment to be used,
 - 4) nature of material (e.g., silty sand),

- 5) any existing sediment grain size and bulk sediment chemistry data for the proposed or any nearby projects,
- 6) information on the location and nature of municipal or industrial discharges and occurrences of any contaminant spills in or near the project area,
- 7) location of the disposal site (include locus sheet),
- 8) shellfish survey, and
- 9) sediment testing, including physical, chemical and biological testing. For projects proposing open water disposal, applicants are encouraged to contact the Corps as early as possible regarding sampling and testing protocols.

The Corps may request additional information. Dredging applicants may be required to conduct a shellfish and/or eel grass survey and sediment testing, including physical, chemical and biological testing. Sediment sampling and testing plans should be prepared or approved by the Corps before the samples are collected.

STATE-FEDERAL SCREENING PROCEDURES:

The Corps intends to utilize the application information required by the State for its regulatory program to the maximum extent practicable and the Corps normally will not be interacting with an applicant who is concurrently making application to the DEP or LURC. Projects not regulated by the State, but needing Corps of Engineers approval, **must apply directly to the Corps**. The joint screening meeting for Category II projects will occur regularly at the Corps or State of fices and will involve representatives from the DEP, the Corps, the U.S. Environmental Protection Agency, the U.S. Fish and Wildlife Service, and the National Marine Fisheries Service.

The Corps and Federal Resource Agencies will classify the project within the State's review period, not to exceed 60 days, as: 1) approvable under the PGP as proposed; 2) needs additional information, including possible project modification, mitigation or other special conditions to minimize impacts; or 3) exceeds the terms or conditions of the PGP, including the minimal effects requirement, and an individual permit review will be required. In addition, the Corps retains the ability to exercise its discretionary authority and require an individual permit, irrespective of whether the terms and conditions of this general permit are met, based on concerns for the aquatic environment or any factor of the public interest (see special condition 4 on Discretionary Authority). All Category II projects must receive written approval from the Corps before work can proceed. If the project is not approvable as proposed, the DEP, LURC, or the Corps will contact the applicant to discuss the concerns raised. If the applicant is unable to resolve the concerns, the Corps, independently or at the request of the Federal Resource Agencies, will require an individual permit for the project. The applicant will be notified of this in writing, along with information about submitting the necessary application materials. The comments from the Federal Resource Agencies to the Corps may be verbal initially, and must be made within 10 working days of the screening meeting. These comments must be confirmed in writing within 10 calendar days of the verbal response if the Resource Agency(ies) will request an individual permit. The Federal Resource Agency's comments must reflect a concern within their area of expertise, state the species or resources that could be impacted by the project, and describe the impacts that either individually or cumulatively will be more than minimal.

MINERALS MANAGEMENT SERVICE (MMS) REVIEW

For Category II projects which involve construction of solid fill structures or discharge of fills along the coast which may extend the coastline or baseline from which the territorial sea is measured, coordination between the Corps and Minerals Management Service (MMS), Continental Shelf (OCS) Survey Group, will be needed (pursuant to the Submerged Lands Act, 43 U.S.C., Section 1301-1315, 33 CFR 320.4(f)). During the screening period, the Corps will forward project information to MMS for their review. MMS will coordinate their determination with the Department of the Interior (DOI) Solicitor's Office. The DOI will have 15 calendar days from the date MMS is in receipt of project information to determine if the baseline will be affected. No notification to the Corps within 15 day review period will constitute a "no affect" determination. Otherwise, the solicitor's notification to the Corps may be verbal but must be followed with a written confirmation within 10 business days from the date of the verbal notification. This procedure will be eliminated if the State of Maine provides a written waiver of interest in any increase in submerged lands caused by a change in the baseline resulting from solid fill structure or fills authorized under this general permit.

D. Corps Authorization: Category III (Individual Permit)

Work that is in the INDIVIDUAL PERMIT category on the attached DEFINITION OF CATEGORIES sheets, or that does not meet the terms and conditions of this general permit, will require an application for an individual permit from the Corps of Engineers (see 33 CFR Part 325.1). The screening procedures outlined above will only serve to delay project review in such cases. The applicant should submit the appropriate application materials (including the Corps application form) at the earliest possible date. General information and application forms can be obtained at (207) 623-8367 (Maine Field Office), (800) 343-4789, or (800) 362-4367 in Massachusetts. Individual water quality certification and coastal zone management consistency concurrence will be required from the State of Maine before Corps permit issuance.

E. Programmatic General Permit Conditions:

The following conditions apply to activities authorized under the PGP, including all Category I (non-reporting) and Category II (reporting - requiring screening) activities:

GENERAL REQUIREMENTS:

1. **Other Permits.** Authorization under this general permit does not obviate the need to obtain other Federal, state, or local authorizations required by law.
2. **Applicability of this general permit shall be evaluated with reference to Federal jurisdictional boundaries.** Applicants are responsible for ensuring that the boundaries used satisfy the federal criteria defined at 33 CFR 328-329.
3. **Minimal Effects.** Projects authorized by this general permit shall have minimal individual and cumulative adverse environmental impacts as determined by the Corps.

4. **Discretionary Authority.** Notwithstanding compliance with the terms and conditions of this permit, the Corps of Engineers retains discretionary authority to require review for an individual permit based on concerns for the aquatic environment or for any other factor of the public interest. This authority is invoked on a case-by-case basis whenever the Corps determines that the potential consequences of the proposal warrant individual review based on the concerns stated above. This authority may be invoked for projects with cumulative environmental impacts that are more than minimal or if there is a special resource or concern associated with a particular project that is not already covered by the remaining conditions of the PGP and that warrants greater review.

Whenever the Corps notifies an applicant that an individual permit may be required, authorization under this general permit is void and no work may be conducted until the individual Corps permit is obtained or until the Corps notifies the applicant that further review has demonstrated that the work may proceed under this general permit.

5. **Single and Complete Projects.** This general permit shall not be used for piecemeal work and shall be applied to single and complete projects. All components of a single project and/or all planned phases of multi-phased projects shall be treated together as constituting one single and complete project (e.g., subdivisions should include all work such as roads, utilities, and lot development). This general permit shall not be used for any activity that is part of an overall project for which an individual permit is required.

NATIONAL CONCERNS:

6. **St. John/St. Croix Rivers.** This covers work within the Saint John and Saint Croix River basins that requires approval of the International Joint Commission. This includes any temporary or permanent use, obstruction or diversion of international boundary waters which could affect the natural flow or levels of waters on the Canadian side of the line, as well as any construction or maintenance of remedial works, protective works, dams, or other obstructions in waters downstream from boundary waters when the activity could raise the natural level of water on the Canadian side of the boundary.
7. **Historic Properties.** Any activity authorized by this general permit shall comply with Section 106 of the National Historic Preservation Act. Information on the location and existence of historic resources can be obtained from the Maine Historic Preservation Commission and the National Register of Historic Places. Federally recognized tribes (Penobscots, Passamaquoddys, Micmacs, and Maliseets) may know of the existence of other sites that may be of significance to their tribes. See page 14 for historic properties contacts.

Applicants with projects which will undergo the screening process (Category II) shall submit a copy of their application materials, with the name and address of the applicant clearly indicated, to the Maine Historic Preservation Commission, 55 Capitol Street, State House Station 65, Augusta, Maine 04333, and to the applicable tribe(s) to be reviewed for the presence of historic and/or archaeological resources in the permit area that may be affected by the proposed work. The Corps will then be notified by the Commission and/or

Tribe within 10 days if there are State and/or tribal concerns that the proposed work will have an effect on historic resources. The applicant should include with their application to the State or the Corps either a copy of their cover letter or a statement of having sent their application material to the Commission and Tribe(s).

If the permittee, either prior to construction or during construction of the work authorized herein, encounters a previously unidentified archaeological or other cultural resource, within the area subject to Department of the Army jurisdiction, that might be eligible for listing in the National Register of Historic Places, he/she shall stop work and immediately notify the District Engineer and the Maine Historic Preservation Commission and/or applicable Tribe(s).

8. **National Lands.** Activities authorized by this general permit shall not impinge upon the value of any National Wildlife Refuge, National Forest, or any area administered by the National Park Service.

9. **Endangered Species.** No activity is authorized under this general permit which

- may affect a threatened or endangered species or a species proposed for such designation as identified under the Federal Endangered Species Act (ESA),
- is likely to destroy or adversely modify the critical habitat or proposed critical habitat of such species,
- would result in a 'take' of any threatened or endangered species of fish or wildlife, or
- would result in any other violation of Section 9 of the ESA protecting threatened or endangered species of plants.

Applicants shall notify the Corps if any listed species or critical habitat, or proposed species or critical habitat, is in the vicinity of the project and shall not begin work until notified by the District Engineer that the requirements of the Endangered Species Act have been satisfied and that the activity is authorized. Information on the location of threatened and endangered species and their critical habitat can be obtained from the U.S. Fish and Wildlife Service and National Marine Fisheries Service (addresses attached, page 14).

10. **Essential Fish Habitat.** As part of the PGP screening process, the Corps will coordinate with the National Marine Fisheries Service (NMFS) in accordance with the 1996 amendments to the Magnuson-Stevens Fishery and Conservation Management Act to protect and conserve the habitat of marine, estuarine and anadromous finfish, mollusks, and crustaceans. This habitat is termed "essential fish habitat (EFH)", and is broadly defined to include "those waters and substrate necessary to fish for spawning, breeding, feeding, or growth to maturity." Applicants may be required to describe and identify potential impacts to EFH based upon the location of the project, the activity proposed, and the species present. Conservation recommendations made by NMFS will normally be included as a permit requirement by the Corps. Information on the location of EFH can be obtained from the NMFS regulations (50 CFR Part 600) (address listed on page 14) and on their web site (<http://www.nero.nmfs.gov/ro/doc/webintro.html>).

The EFH designation for Atlantic salmon includes all aquatic habitats in the watershed of the following rivers and streams, including all tributaries to the extent that they are currently or were historically accessible for salmon migration:

St. Croix River	Pleasant River	Union River
Boyden River	Narraguagus River	Ducktrap River
Dennys River	Tunk Stream	Sheepscot River
Hobart Stream	Patten Stream	Kennebec River
Aroostook River	Orland River	Androscoggin River
East Machias River	Penobscot River	Presumpscot River
Machias River	Passagassawaukeag River	Saco River

11. **Wild and Scenic Rivers.** Any activity that occurs in a component of, or within 0.25 mile up or downstream of the main stem or tributaries of a river segment of the National Wild and Scenic River System, **must be reviewed by the Corps under the procedures of Category II of this general permit regardless of size of impact.** This condition applies to both designated wild and scenic rivers and rivers designated by Congress as study rivers for possible inclusion while such rivers are in an official study status. The Corps will consult with the National Park Service (NPS) with regard to potential impacts of the proposed work on the resource values of the Wild and Scenic River. The culmination of this coordination will be a determination by the NPS and the Corps that the work: (1) may proceed as proposed; (2) may proceed with recommended conditions; or (3) could pose a direct and adverse effect on the resource values of the river and an individual permit is required. If pre-application consultation between the applicant and the NPS has occurred whereby the NPS has made a determination that the proposed project is appropriate for authorization under this PGP (with respect to wild and scenic river issues), this determination should be furnished to the Corps with submission of the application. The address of the NPS can be found on Page 14 of this permit. *National Wild/Scenic Rivers System (Designated River in Maine) as of 5/2/00:* Allagash River beginning at Telos Dam continuing to Allagash checkpoint at Eliza Hole Rapids, approximately 3 miles upstream of the confluence with the St. John River. Length = 92 miles

12. **Federal Navigation Project.** Any structure or work that extends closer to the horizontal limits of any Corps navigation project than a distance of three times the project's authorized depth (see attached map following page 16 for locations of these projects) shall be subject to removal at the owner's expense prior to any future Corps dredging or the performance of periodic hydrographic surveys.

13. **Navigation.** There shall be no unreasonable interference with navigation by the existence or use of the activity authorized herein and no attempt shall be made by the permittee to prevent the full and free use by the public of all navigable waters at or adjacent to the activity authorized herein.

The permittee understands and agrees that, if future operations by the United States require the removal, relocation, or other alteration, of the structure or work herein authorized, or if, in the opinion of the Secretary of the Army or his authorized representative, said structure

or work shall cause unreasonable obstruction to the free navigation of the navigable waters, the permittee will be required, upon due notice from the Corps of Engineers, to remove, relocate, or alter the structural work or obstructions caused thereby, without expense to the United States. No claim shall be made against the United States on account of any such removal or alteration.

14. **Federal Liability.** In issuing this permit, the Federal Government does not assume any liability for the following: (a) damages to the permitted project or uses thereof as a result of other permitted or unpermitted activities or from natural causes; (b) damages to the permitted project or uses thereof as a result of current or future activities undertaken by or on behalf of the United States in the public interest; (c) damages to persons, property, or to other permitted or unpermitted activities or structures caused by the activity authorized by this permit; (d) design or construction deficiencies associated with the permitted work; (e) damage claims associated with any future modification, suspension, or revocation of this permit.

MINIMIZATION OF ENVIRONMENTAL IMPACTS:

15. **Minimization.** Discharges of dredged or fill material into waters of the United States shall be avoided and minimized to the maximum extent practicable, regardless of review category.
16. **Work in Wetlands.** Heavy equipment working in wetlands shall be avoided if possible, **and if required, shall be placed on mats or other measures taken** to minimize soil and vegetation disturbance. Disturbed areas in wetlands shall be restored to preconstruction contours and conditions upon completion of the work.
17. **Temporary Fill.** Temporary fill in waters and wetlands authorized by this general permit (e.g., access roads, cofferdams) shall be properly stabilized during use to prevent erosion. Temporary fill in wetlands shall be placed on geotextile fabric laid on existing wetland grade. Temporary fills shall be disposed of at an upland site, suitably contained to prevent erosion and transport to a waterway or wetland. Temporary fill areas shall be restored to their approximate original contours but not higher. No temporary fill shall be placed in waters or wetlands unless specifically authorized by the Corps.
18. **Sedimentation and Erosion Control.** Adequate sedimentation and erosion control management measures, practices and devices, such as phased construction, vegetated filter strips, geotextile silt fences or other devices, shall be installed and properly maintained to reduce erosion and retain sediment on-site during and after construction. They shall be capable of preventing erosion, of collecting sediment, suspended and floating materials, and of filtering fine sediment. These devices shall be removed upon completion of work and the disturbed areas shall be stabilized. The sediment collected by these devices shall be removed and placed at an upland location in a manner that will prevent its later erosion into a waterway or wetland. All exposed soil and other fills shall be permanently stabilized at the earliest practicable date.

19. Waterway Crossings.

- (a) All temporary and permanent crossings of waterbodies shall be suitably culverted, bridged, or otherwise designed to withstand and to prevent the restriction of high flows, to maintain existing low flows, and to not obstruct the movement of aquatic life indigenous to the waterbody beyond the actual duration of construction.
- (b) Temporary bridges, culverts, or cofferdams shall be used for equipment access across streams (NOTE: areas of fill and/or cofferdams must be included in total waterway/wetlands impacts to determine applicability of this general permit).
- (c) For projects that otherwise meet the terms of Category I, instream construction work shall be conducted during the low flow period July 15 - October 1 in any year. Projects that are not to be conducted during that time period are ineligible for Category I and shall be screened pursuant to Category II, regardless of the waterway and wetland fill and/or impact area.

20. Discharge of Pollutants. All activities involving any discharge of pollutants into waters of the United States authorized under this general permit shall be consistent with applicable water quality standards, effluent limitations, standards of performance, prohibitions, and pretreatment standards and management practices established pursuant to the Clean Water Act (33 U.S.C. 1251) and applicable state and local laws. If applicable water quality standards, limitations, etc., are revised or modified during the term of this permit, the authorized work shall be modified to conform with these standards within six months of the effective date of such revision or modification, or within a longer period of time deemed reasonable by the District Engineer in consultation with the Regional Administrator of the Environmental Protection Agency. Applicants may presume that state water quality standards are met with issuance of the 401 Water Quality Certification.

21. Spawning Areas. Discharges into known 1) fish and shellfish spawning or nursery areas; and 2) amphibian and waterfowl breeding areas, during spawning or breeding seasons shall be avoided, and impacts to these areas shall be avoided or minimized to the maximum extent practicable during all times of year.

22. Storage of Seasonal Structures. Coastal structures such as pier sections and floats that are removed from the waterway for a portion of the year shall be stored in an upland location located above mean high water and not in tidal marsh.

23. Environmental Values. The permittee shall make every reasonable effort to carry out the construction or operation of the work authorized herein in a manner so as to maintain as much as is practicable, and to minimize any adverse impacts on, existing fish and wildlife and natural environmental values.

24. Protection of Vernal Pools. Impacts to uplands in proximity (within 500 feet) to the vernal pools referenced in DEFINITIONS OF CATEGORIES shall be minimized to the maximum extent possible.

PROCEDURAL CONDITIONS:

25. **Cranberry Development Projects.** For Cranberry development projects authorized under the PGP, the following conditions apply:

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1. If a cranberry bog is abandoned for any reason, the area must be allowed to convert to natural wetlands unless an individual permit is obtained from the Corps of Engineers allowing the discharge of fill for an alternate use.
2. No stream diversion shall be allowed under this permit.
3. No impoundment of perennial streams shall be allowed under this permit.
4. The project shall be designed and constructed to not cause flood damage on adjacent properties.

26. **Inspections.** The permittee shall permit the District Engineer or his authorized representative(s) to make periodic inspections at any time deemed necessary in order to ensure that the work is being performed in accordance with the terms and conditions of this permit. The District Engineer may also require post-construction engineering drawings for completed work, and post-dredging survey drawings for any dredging work. **To facilitate these inspections, the attached work notification form should be filled out and returned to the Corps for all Category II projects.**

27. **Maintenance.** The permittee shall maintain the work or structures authorized herein in good condition, including maintenance, to ensure public safety. Dredging projects: note that this does not include maintenance of dredging projects. Maintenance dredging is subject to the review thresholds described on the attached DEFINITION OF CATEGORIES sheets and/or any conditions included in a written Corps authorization.

28. **Property Rights.** This permit does not convey any property rights, either in real estate or material, or any exclusive privileges, nor does it authorize any injury to property or invasion of rights or any infringement of federal, state, or local laws or regulations. **If property associated with work authorized by the PGP is sold, the PGP authorization is automatically transferred to the new property owner. The new property owner should provide this information to the Corps in writing. No acknowledgement from the Corps is necessary.**

29. **Modification, Suspension, and Revocation.** This permit may be either modified, suspended, or revoked, in whole or in part, pursuant to the policies and procedures of 33 CFR 325.7 and any such action shall not be the basis for any claim for damages against the United States.

30. **Restoration.** The permittee, upon receipt of a notice of revocation of authorization under this permit, shall restore the wetland or waterway to its former condition without expense to the United States and as directed by the Secretary of the Army or his authorized representative. If the permittee fails to comply with such a directive, the Secretary or his designee may restore the wetland or waterway to its former condition, by contract or otherwise, and recover the cost from the permittee.

31. **Special Conditions.** The Corps, independently or at the request of the Federal Resource Agencies, may impose other special conditions on a project authorized pursuant to this general permit that are determined necessary to minimize adverse environmental effects or based on any other factor of the public interest. Failure to comply with all conditions of the authorization, including special conditions, will constitute a permit violation and may subject the permittee to criminal, civil, or administrative penalties or restoration.
32. **False or Incomplete Information.** If the Corps makes a determination regarding the eligibility of a project under this permit and subsequently discovers that it has relied on false, incomplete, or inaccurate information provided by the permittee, the permit shall not be valid and the government may institute appropriate legal proceedings.
33. **Abandonment.** If the permittee decides to abandon the activity authorized under this general permit, unless such abandonment is merely the transfer of property to a third party, he/she must restore the area to the satisfaction of the District Engineer.
34. **Enforcement cases.** This general permit does not apply to any existing or proposed activity in Corps jurisdiction associated with an on-going Corps of Engineers or Environmental Protection Agency enforcement action until such time as the enforcement action is resolved or the Corps determines that the activity may proceed independently without compromising the enforcement action. The Corps may choose not to accept applications or issue permits to any applicant with outstanding violations.
35. **Emergency situations.** This PGP can be used to authorize the repair, rehabilitation, or replacement of those structures destroyed by storms, floods, fire or other discrete unexpected and catastrophic event. In such situations and if the work exceeds Category I limitations, if applicant applies to the Corps within 30 days of the event, the Corps will attempt to contact the resource agencies for their approvals but, if unable to contact them, will issue an emergency permit and review them after-the-fact with the agencies at the next joint processing meeting. Proposed work submitted more than 30 days after the emergency will go through the standard PGP procedures.

DURATION OF AUTHORIZATION/GRANDFATHERING:

36. **Duration of Authorization.** Activities authorized under this general permit that have commenced (i.e., are under construction) or are under contract to commence in reliance upon this authorization will remain authorized provided the activity is completed within twelve months of the date of the general permit's expiration, modification, or revocation, unless discretionary authority has been exercised on a case-by-case basis to modify, suspend, or revoke the authorization in accordance with 33 CFR 325.2 (e)(2). Activities completed under the authorization of the general permit that was in effect at the time the activity was completed will continue to be authorized by the general permit.

37. Previously Authorized Activities.

- (a) Activities which have commenced (i.e., are under construction or are under contract to commence) prior to the issuance date of this general permit, in reliance upon the terms and conditions of the non-reporting category of the previous Maine PGP shall remain authorized provided the activity is completed within twelve months of the date of issuance of this general permit, unless discretionary authority has been exercised on a case-by-case basis to modify, suspend, or revoke the authorization in accordance with special condition 4. The applicant must be able to document to the Corps satisfaction that the project was under construction or contract by the appropriate date.
- (b) Projects that have received written verification or approval from the Corps, based on applications made to the Corps prior to issuance of this general permit, for the previous Maine SPGP and PGP, Nationwide permits, regional general permits, or letters of permission shall remain authorized as specified in each authorization.
- (c) This general permit does not affect activities authorized pursuant to 33 CFR Part 330.3 (activities occurring before certain dates).

For DISTRICT ENGINEER Christine Gedfrey DATE 7 / 26 / 00

CONTACTS FOR MAINE PROGRAMMATIC GENERAL PERMIT:

U.S. Army Corps of Engineers
Maine Project Office
675 Western Avenue #3
Manchester, Maine 04351
207-623-8367
Fax # 207-623-8206

Federal Endangered Species
U.S. Fish and Wildlife Service
Maine Field Office
1033 South Main Street
Old Town, Maine 04468
207-827-5938
Fax # 207-827-6099

Wild and Scenic Rivers
National Park Service
North Atlantic Region
15 State Street
Boston, MA 02109
617-223-5203

Maine Historic Preservation Commission
55 Capitol Street
State House Station 65
Augusta, Maine 04333
207-287-2132
Fax # 207-287-2335
Aroostook Band of Micmacs
P.O. Box 772
Presque Isle, Maine 04769
207-764-1972
Fax # 207-764-7667

Passamaquoddy Tribe of Indians
Pleasant Point Reservation
Attn: Tribal Council
P.O. Box 343
Perry, Maine 04667
207-853-2600
Fax # 207-853-6039

*Federal Endangered Species and Essential
Fish Habitat*
National Marine Fisheries Service
One Blackburn Drive
Gloucester, Massachusetts 01939
978-281-9102
Fax # 978-281-9301

Houlton Band of Maliseet Indians
Attn: Brenda Commander, Tribal Chief
Route 3 - Box 450
Houlton, Maine 04730
207-532-4273
Fax # 207-532-2660
Passamaquoddy Tribe of Indians
Indian Township Reservation
Attn: Donald Soctomah
P.O. Box 301
Princeton, Maine 04668
207-796-2301
Fax # 207-796-5256

Penobscot Indian Nation
Richard Hamilton, Chief
6 River Road
Indian Island Reservation
Old Town, Maine 04468
(207) 827-7776
Fax # 207-827-1137

*Maine Department of Environmental Protection
(For State Permits and Water Quality
Certifications)*

Natural Resources Division
Bureau of Land and Water Quality Control
State House Station 17
Augusta, Maine 04333
207-287-2111

Southern Maine Regional Office
312 Canco Road
Portland, Maine 04103
201-822-6300

Eastern Maine Regional Office
106 Hogan Road
Bangor, Maine 04401
207-941-4570

Northern Maine Regional Office
1235 Central Drive
Skyway Park
Presque Isle, Maine 04769
207-764-0477

*MaineLand UseRegulation Commission (LURC)
offices*

22 State House Station
Augusta, ME 04333-0022
207-287-2631
800-452-8711 (call to obtain appropriate LURC
of fice)
Fax # 207-287-7439

45 Radar Road
Ashland,ME 04732-3600
207-435-7963
Fax # 207-435-7184

Lakeview Drive
P.O.Box1107
Greenville, ME 04441
207-695-2466
Fax # 207-695-2380

191 Main Street
EastMillinocket,ME 04430
207-746-2244
Fax # 207-746-2243

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(For CZMDeterminations)

State Planning Office
Coastal Program
184 State Street
State House Station 38
Augusta, Maine 04333
207-287- 1009

*Maine Department of Marine Resources
(For Aquaculture Leases)*
McKown Point
Boothbay Harbor, Maine 04575
207-633-9500

(For Submerged Lands Leases)

Maine Department of Conservation
Bureau of Parks and Lands
22 State House Station
207-287-3061

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A. INLAND WETLANDS (WATERS OF THE U.S.) ¹	CATEGORY I	CATEGORY II	INDIVIDUAL PERMIT
(a) NEW FILL/ EXCAVATION DISCHARGES	<p>Less than 4,300 sf inland waterway and /or wetland fill and secondary impacts (e.g., areas drained, flooded or cleared).</p> <p>-- Includes projects covered by a State Tier One permit with no cumulative impacts over 15,000 sf in inland wetlands from previous permits, unauthorized work, and/or other state permits.</p> <p>-- Includes crossing of perennial waterways designated as Essential Fish Habitat (EFH) for Atlantic salmon² if the waterway is crossed with a span and footprints of the span abutments are outside ordinary high water with no more than 4,300 sf of associated wetland impact.</p> <p>-- Includes in-stream work of up to 4,300 sf of fill below ordinary high water in waterways not designated as EFH for Atlantic salmon² and performed in accordance with Maine Permit By Rule standards or a LURC permit.</p>	<p>4,300 sf to 3 acres inland waterway and/or wetland fill and secondary impacts (e.g., areas drained, flooded or cleared).</p> <p>-- Impact area includes all temporary and permanent fill and excavation except for incidental fallback³.</p> <p>-- Includes in-stream work, including crossings (other than a spanned crossing as described in Category I) with any discharge of fill below ordinary high water in perennial waterways designated as EFH for Atlantic salmon².</p> <p>-- Time of year restrictions determined case-by-case.</p>	<p>Greater than 3 acres inland waterway and/or wetland fill and secondary impacts (e.g., areas drained, flooded or cleared).</p> <p>-- Impact area includes all temporary and permanent fill and excavation discharges except for incidental fallback³.</p> <p>In-stream work exceeding Category II limits.</p> <p>If EIS required by the Corps.</p>

¹ Water of the U.S. in inland areas: inland rivers, streams, lakes, ponds and wetlands.

² Essential Fish Habitat for Atlantic salmon includes all aquatic habitats in the watersheds of the following rivers and streams, including all tributaries to the extent that they are currently or were historically accessible for salmon migration: St. Croix, Boyden, Dennys, Hobart Stream, Aroostook, East Machias, Machias, Pleasant, Narraguagus, Tunk stream, Patten Stream, Orland, Penobscot, Passagassawaukeag, Union, Ducktrap, Sheepscot, Kennebec, Androscoggin, Presumpscot and Saco River.

³ The larger the impacts, the more likely an individual permit will be required. Projects involving widening, expansion or impacts to degraded or low value wetlands between 1-3 acres may be approved under Category II, subject to the Federal screening. The Corps recognizes and endorses the DEP Tier 2 upper thresholds of 1 acre. Compensatory mitigation is likely to be required at this level of impact.

	CATEGORY I	CATEGORY II	INDIVIDUAL PERMIT
(a) NEW FILL/ EXCAVATION DISCHARGES	<p>-- Impact area includes all temporary and permanent fill and excavation discharges except for incidental fallback.</p> <p>-- In-stream work limited to July 15 - Oct. 1.</p> <p>-- This category excludes situations when a vernal pool of any size may be impacted, in accordance with the ME DEP definition of vernal pool⁴</p> <p>-- This category excludes work within ¼ mile or a Wild and Scenic River⁵</p> <p>-- This category excludes dams, dikes, or activities involving water withdrawal or water diversion.</p> <p>-- This category excludes work in National Wildlife Refuges.</p>	Proactive restoration projects with any amount of impact can be reviewed under Category II. The Corps, in consultation with State and Federal agencies, must determine that net adverse effects are not more than minimal.	
(b) BANK STABILIZATION PROJECTS	<p>Inland bank stabilization less than 500 ft. long and less than 1 cy fill per linear foot below ordinary high water in ponds, lakes, and waterway not designated as EFH for Atlantic salmon², provided there is no wetland fill.</p> <p>-- In-stream work limited to July 15 - Oct. 1.</p>	<p>Inland bank stabilization in ponds, lakes, and waterways not designated as EFH for Atlantic salmon² which exceeds Category I limits.</p> <p>Inland bank stabilization of any size below ordinary high water in waterways designed as EFH for Atlantic salmon².</p> <p>-- Other stabilization exceeding Category I.</p>	
(C) REPAIR AND MAINTENANCE OF AUTHORIZED FILLS	Repair or maintenance of existing, currently serviceable, authorized fills with no substantial expansion or change in use.	Replacement of non-serviceable fills, or repair or maintenance of serviceable fills with expansion of any amount up to 1 acre, or with a change in use.	Replacement of non-serviceable fills, or repair or maintenance of serviceable fills with greater than 1 acre of expansion.

⁴ Vernal Pool: Naturally-occurring, or intentionally created for the purposes of compensatory mitigation, temporary to permanent bodies of water occurring in shallow depressions that fill during the spring and fall and may dry during the summer. Vernal pools have no permanent or viable populations of predatory fish. Vernal pools provide the primary breeding habitat for wood frogs, spotted salamanders, blue-spotted salamanders, and fairy shrimp, and provide habitat for other wildlife including several endangered and threatened species.

⁵ National Wild/Scenic Rivers System (Designated River in Maine): Allagash River beginning at Telos Dam continuing to Allagash checkpoint at Eliza Hole Rapids, approximately 3 miles upstream of the confluence with the St. John River. Length = 92 miles.

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B. TIDAL WATERS AND NAVIGABLE WATERS ⁶	CATEGORY I	CATEGORY II	INDIVIDUAL PERMIT
(a) FILL		Up to 1 acre waterway or wetland fill and secondary impacts (e.g., areas drained, flooded or cleared). Includes temporary and permanent waterway fill. -- Temporary tidal marsh impacts up to 1 acre. -- Permanent tidal marsh, mudflat, or vegetated shallows 7 fill up to 1,000 sf. -- Proactive restoration projects with any amount of impact can be reviewed under Cat. II. The Corps, in consultation with State and Federal agencies, must determine that net adverse effects are not more than minimal.	Greater than 1 acre waterway fill and secondary impacts (e.g., areas drained, flooded or cleared). Includes -- Temporary tidal marsh impacts over 1 acre. -- Permanent tidal marsh, mudflat, or vegetated shallows 7 fill over 1,000 sf.
(b) REPAIR AND MAINTENANCE WORK	Repair or maintenance of existing, currently serviceable, authorized structure or fills with no substantial expansion or change in use. -- Work must be in same footprint as original structure or fill	Repair or replacement of any non-serviceable structures or fill, or repair or maintenance of serviceable fills with expansion of any amount up to 1 acre, or with a change in use.	Replacement of non-serviceable structures or fill or repair or maintenance of serviceable structure or fill with expansion greater than 1 acre.

6 Navigable Waters: waters that are subject to the ebb and flow of the tide and Federally designated navigable waters (Penobscott River to Medway, Kennebec River to Moosehead Lake, and the portion of Umbagog Lake in Maine).

7 Vegetated Shallows: subtidal areas that support rooted aquatic vegetation such as eelgrass.

	CATEGORY I	CATEGORY II	INDIVIDUAL PERMIT
(c) DREDGING	Maintenance dredging of less than 1,000 cy with upland disposal. -- Proper siltation controls used -- Limited to work between November 1 and January 15. -- No impact to special aquatic sites ⁸	Maintenance dredging of greater than 1,000 cy, new dredging of up to 25,000 cy, or projects that do not meet Category I. Disposal includes upland, open water or beach nourishment (above mean high water), only if material is determined suitable.	Maintenance dredging (any amount) in or affecting special aquatic sites ⁷ . See B(a) above for dredge disposal in wetlands or water. New dredging greater than 25,000 cy or any amount in or affecting special aquatic sites ⁷ .
(d) MOORINGS	-- Private, non-commercial, non-rental single boat moorings not associated with any boating facility? ⁹ provided not located in a Federal Navigation Project, there is no interference with navigation, it is not located in vegetated shallows ⁶ , and it is within ¼ mile of the owner's residence or a public access point ¹⁰ . -- Minor relocation or previously authorized mooring and moored floats consistent with Harbormaster recommendations, provided it is also consistent with local regulations, is not located in vegetated shallows, and does not interfere with navigation.	Moorings that do not meet the terms of Category I (e.g., rental or service moorings) and moorings that meet the terms of Category I that are located in a Federal anchorage.	Moorings within the horizontal limits, or with moored vessels that extend, into the horizontal limits of a Federal Navigation Project, except those in Federal anchorages under Category II.

⁸Special Aquatic Sites: include wetlands and salt marsh, mudflats, riffles and pools, and vegetated shallows.

? Boating Facilities: facilities that provide, rent, or sell mooring space, such as marinas, yacht, clubs, boat clubs, boat yards, town facilities, dockominiums, etc.

¹⁰ Cannot be at a remote location to create a convenient transient anchorage.

	CATEGORY I	CATEGORY II	INDIVIDUAL PERMIT
(e) PILE-SUPPORTED STRUCTURES AND FLOATS	Reconfiguration of existing authorized docks, provided structures are not positioned over vegetated shallows or salt marsh and provided floats are supported off substrate at low tide. No dredging, addition slips or expansion allowed.	Private piers and floats for navigational access to waterway (seasonal and permanent).	Structures, piers or floats that extend, or with docked/moored vessels that extend, into the horizontal limits of a Federal Navigation Project. Structures, including piers and floats, associated with a new or previously unauthorized boating facility ⁸ .
(f) MISCELLANEOUS	<ul style="list-style-type: none"> -- Temporary buoys, markers, floats, etc., for recreational use during specific events, provided they are removed within 30 days after use is discontinued. -- Coast Guard approved aids to navigation. -- Oil spill clean-up temporary structures or fill. -- Fish/wildlife harvesting structures/fill (as defined by 33 CFR 330, App. A-4) -- Scientific measurement devices and survey activities such as exploratory drilling, surveying or sampling. -- Shellfish seeding (brushing the flats) projects¹¹. -- Does <u>not</u> include oil or gas exploration and fills for roads or construction pads. -- This category excludes work in National Wildlife Refuges. 	<ul style="list-style-type: none"> -- Structures or work in or affecting tidal or navigable waters that are not defined under any or the previous headings. Includes, but is not limited to, utility lines, aerial transmission lines, pipelines, outfalls, boat ramps, bridge fills/abutments, etc. -- Shellfish/finfish (other than Atlantic salmon), or other aquaculture facilities which are consistent with the Corps revised standard siting requirements and standard permit conditions dated 7/6/94, or as revised. 	If EIS required by Corps.

¹¹ Brushing the flats: the placement of tree boughs, wooden lath structures, or small-mesh fencing on mudflats for the purpose of enhancing recruitment of soft-shell clams (*Mya arenaria*).